



River Islands

at Lathrop

**2020 Urban Design Concept Amendment
for Phase 2 of River Islands**

May 2021

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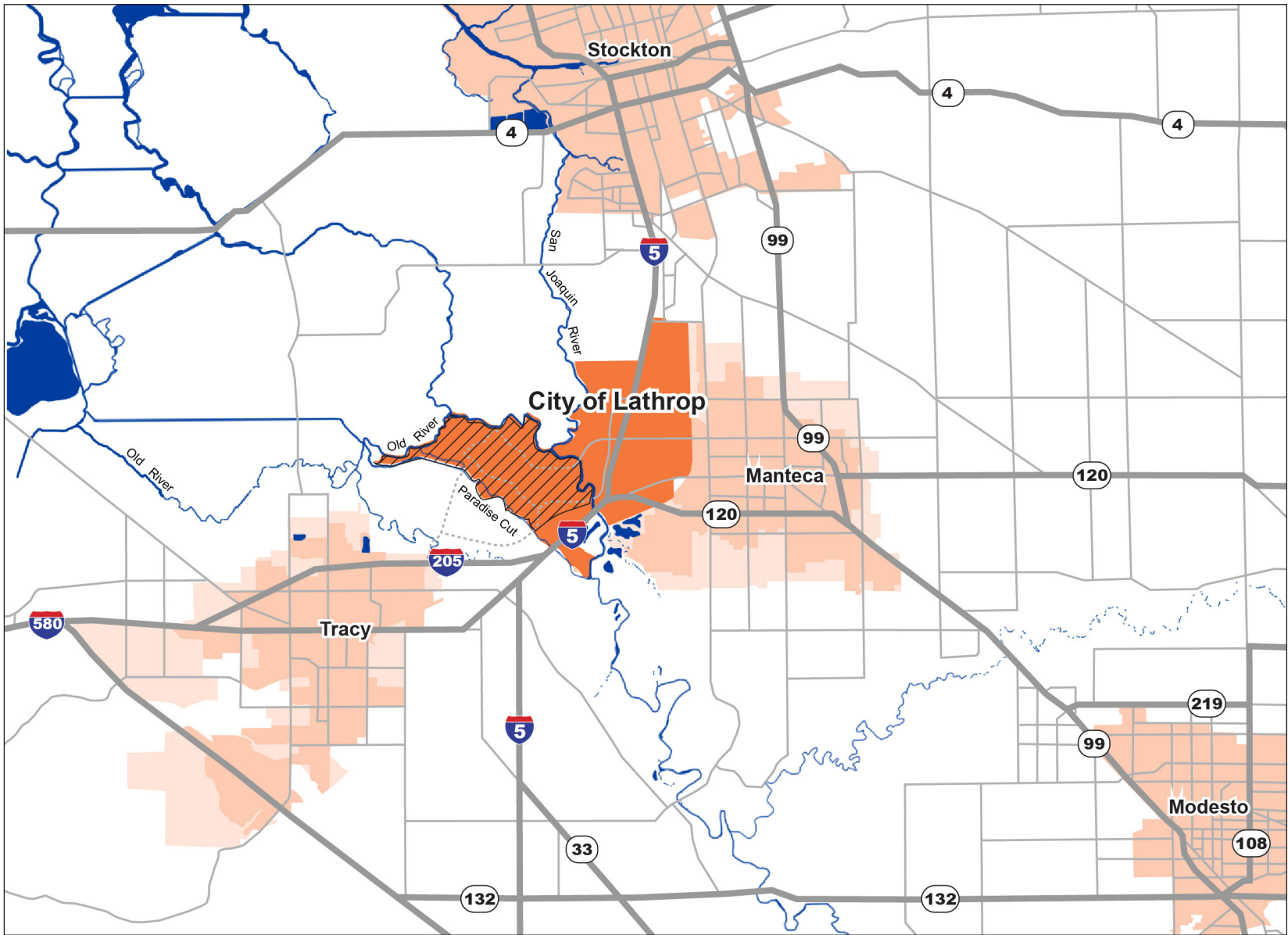


Figure I-1: Vicinities Map (Hatched area indicates the River Islands Project)

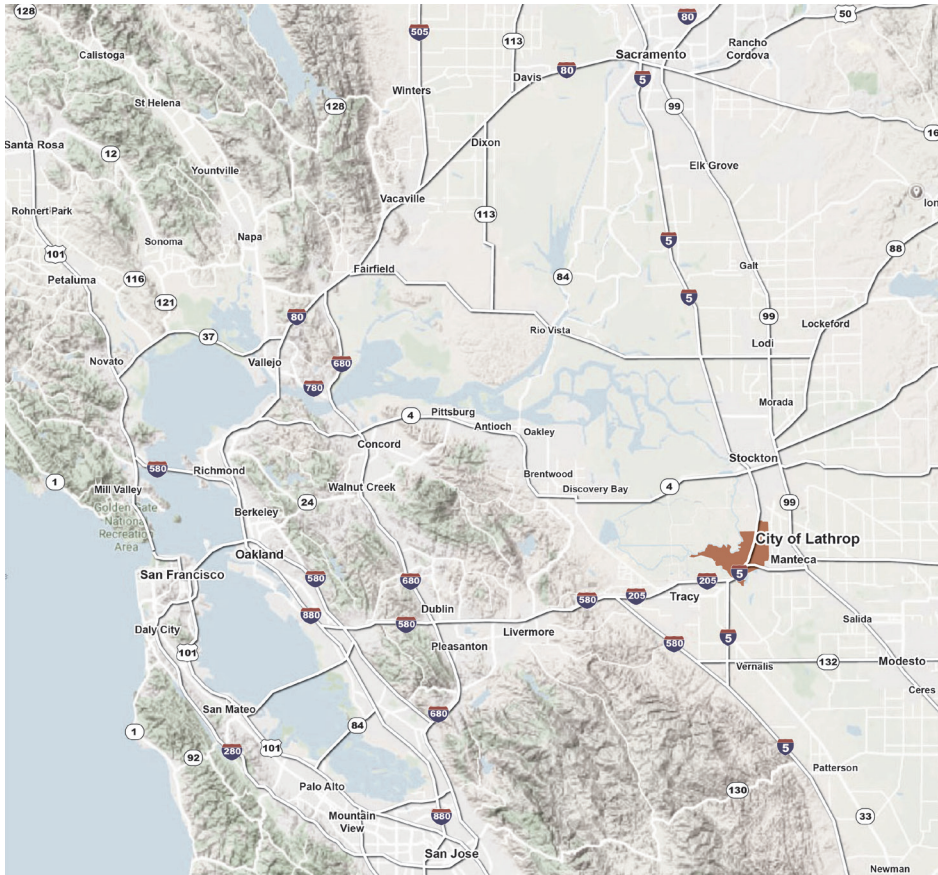


Figure I-2: Location Map (Regional Context)

INTRODUCTION

River Islands is a planned community located in Lathrop, on an island in the San Joaquin River System. Encompassing approximately 5,000 acres, River Islands will eventually become a special place to live, work and recreate. The River Islands community will consist of more than 15,000 homes, vibrant town centers, an employment center with thousands of jobs for local residents, a Valley Link transit connection to the Bay Area, and an extensive and unique open space system. Figure I-1 shows the location of Lathrop in relation to the Central Valley and the Bay Area. Figure I-2 shows the relationship of River Islands to the rest of Lathrop, as well as other surrounding cities.

A. PURPOSE & INTENT

The 2020 Urban Design Concept (UDC) Amendment for Phase 2 of River Islands ("Phase 2 of River Islands UDC" or "Phase 2 UDC" hereafter) provides a vision for the community and describes the design concepts and physical framework that apply to the Phase 2 area of the River Islands community. Its primary purpose is to control the quality of development within Phase 2 of River Islands throughout all stages. It complements the development standards contained in the 2020 West Lathrop Specific Plan (WLSP) Amendment and provides conceptual guidance to the more detailed design standards of subsequent documents (e.g. Neighborhood Development Plans, Architectural Design Guidelines & Development Standards) that can be used by the Stewart Tract Design Review Committee (STDRC) and the City to evaluate design proposals. It is also intended to be a tool that planners and designers can use to formulate specific development proposals for Phase 2 of River Islands.

Although the primary focus of the Phase 2 UDC is the design of the public realm — such as open space, landscape, and architecture — it is also intended to provide guidance to development on private property. Especially important in this respect is the architecture and landscape of commercial buildings, and the character of residential neighborhoods.

B. RELATIONSHIP TO WEST LATHROP SPECIFIC PLAN

The 2020 WLSP Amendment provides the authority under which the Phase 2 UDC has been prepared. As described in the WLSP, each Specific Plan subarea is required to have a UDC that provides guidelines for development. Consistent with the Plan Area defined by the 2020 WLSP Amendment, this UDC applies only to the Phase 2 area of River Islands. This UDC should be used in conjunction with the development standards found in Chapter 4, "Community Design" of the 2020 WLSP Amendment.

C. STEWART TRACT DESIGN REVIEW COMMITTEE

The Stewart Tract Design Review Committee (STDRC) was created by the authority of the WLSP and consists of three members, including a City of Lathrop representative as well as experienced design professionals. The STDRC reviews development proposals to ensure compliance with the UDC and the overall intent of the WLSP, and recommends changes, if necessary. The STDRC is the primary design advisor to the City of Lathrop Planning Commission and City Council for the Stewart Tract, including Phase 2 of River Islands.

The purpose and responsibilities of the STDRC is provided in the Lathrop Municipal Code under *Section 17.61.170*, including the authority to recommend exceptions or variances for development proposals, and revisions to the Phase 2 of River Islands UDC. Revisions to the Phase 2 UDC as part of recommending a development proposal must still be consistent with the WLSP.

D. LANGUAGE

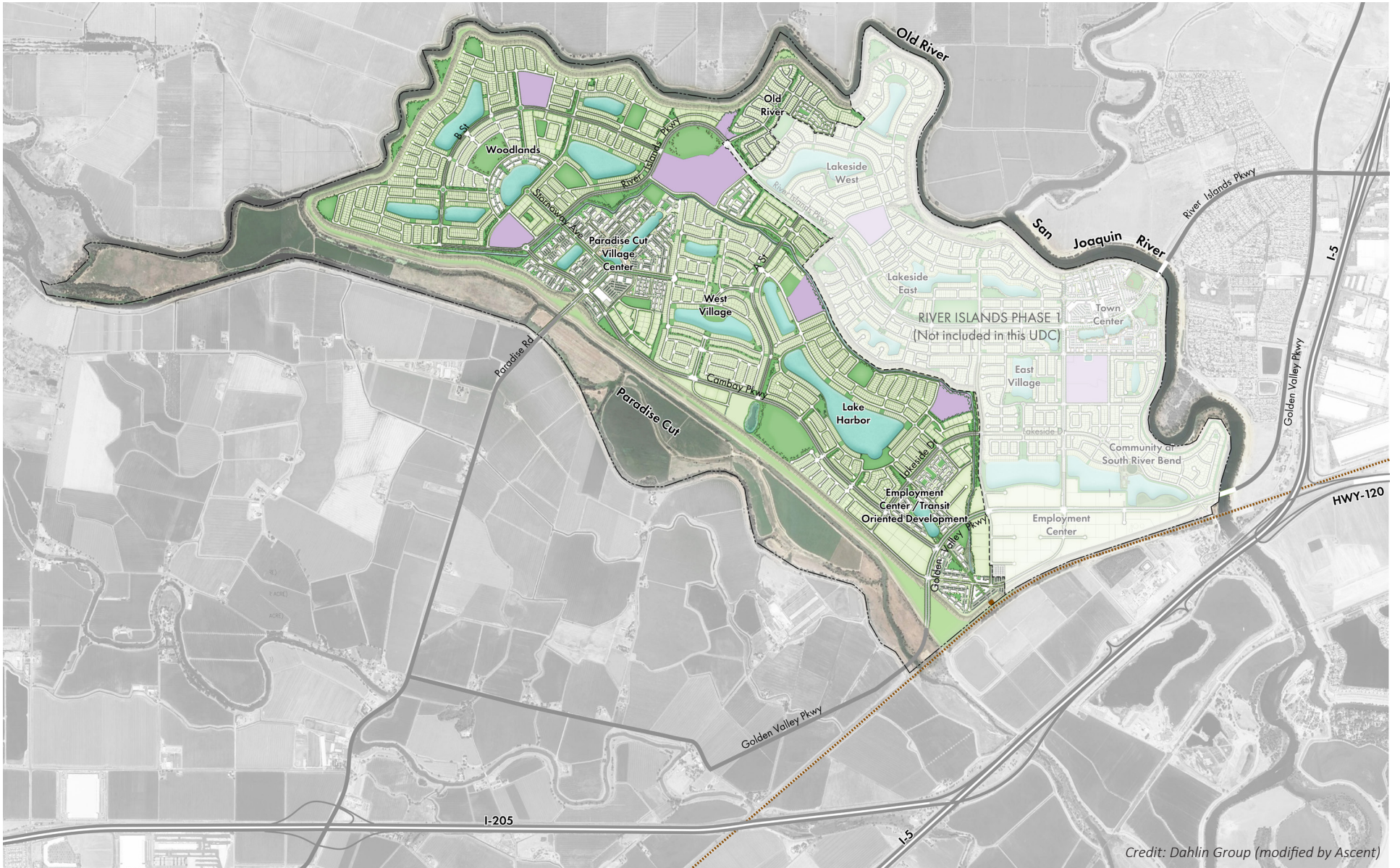
The Phase 2 of River Islands UDC uses language to assist the STDRC in reviewing design proposals. The words, "shall", "will", and "must" are to be considered requirements. All development standards intended to supplement the 2020 WLSP's and City of Lathrop's zoning requirements use this language.

The word "should" indicates that the guideline is required under most circumstances, but that special conditions might apply that prevent application as described in the UDC. The words "may" and "can" indicate possible design solutions that are acceptable, but not required.

E. ORGANIZATION OF THE DOCUMENT

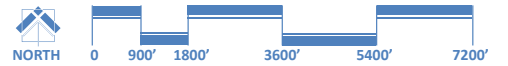
The Phase 2 UDC includes two major sections: Community Structure (Chapters 1 to 3) and District Urban Design Framework (Chapter 4). The Community Structure section describes the overall urban design systems that apply to the entire Phase 2 area of River Islands. It includes descriptions of major urban design systems (e.g., open space and landscape design). Each system is described with text and drawings that illustrate its location and extent. Complementary sketches, diagrams, cross sections and other graphic materials further illustrate the UDC's design intent.

The District Urban Design Framework section addresses the particular conditions of each of the River Islands districts. As shown in Figure I-3, there are twelve River Islands districts. Town Center, Employment Center, Community at South River Bend, East Village, Lakeside East, Lakeside West districts are located in Phase 1 of River Islands. Paradise Cut Village Center, Employment Center/Transit Oriented Development, Woodlands, Old River, West Village, and Lake Harbor districts are located in Phase 2, and are the main focus of this UDC. The District Urban Design Framework section contains more detailed physical design criteria for each of the Phase 2 districts.



Credit: Dahlin Group (modified by Ascent)

Figure I-3: Phase 2 of River Islands Overall Illustrative Development Plan





01

**OPEN SPACE AND
RECREATION SYSTEM**



CH.1: OPEN SPACE & RECREATION SYSTEM

Open Space plays an important role in establishing the community character and is an especially significant component of River Islands. It provides a direct connection to the natural landscape of the San Joaquin River Delta and establishes River Islands as a delta community. It also provides the structural framework for the community, giving it a coherent, visual form. Finally, open space provides space for recreation and leisure that enhances the life of every resident.

The San Joaquin River system surrounds the community and forms the largest component of open space in River Islands. This open space anchors River Islands to the San Joaquin River Delta region, providing a natural and aesthetically attractive edge. It also provides space for flood storage, wildlife habitat and passive recreation opportunities.

The interior lakes and waterway system within Phase 2 of River Islands is the second largest open space component, creating aesthetic focal points for the community. It also provides storage for stormwater, wetland areas to clean and polish runoff and lake water, and water-edge trails for walking, bicycling, and passive recreational use.

Community parks are the most important part of the active recreation system within Phase 2 of River Islands. These 15– to 30–acre facilities feature play fields, game courts and other specialized recreation facilities.

The neighborhood park, pocket park, and linear park system provides convenient access to passive and active recreation sites, and is within a few hundred feet of nearly every home in River Islands. The form and character of the park system varies according to the focus of the neighborhood: neighborhoods with frontage on a water body are different from those located at the heart of a village.

Knitting River Islands together is a system of trails for pedestrians and bicycles that connects all major elements. The tree-lined public street system plays a role in this as well by providing additional visual and physical linkages. Figure 1.1 illustrates the open space system components in Phase 2 of River Islands.

Private recreation facilities may also be developed in Phase 2 of River Islands, as has been provided in Phase 1 (Sunset Point). They will provide residents with special recreational opportunities, such as tennis courts, spas, or swimming lagoons.

Within the following sections, a brief summary identifies the purpose, program, typical facilities and control authority for each open space element. The "Purpose" section lists the major goals that are to be accomplished by the particular type of open space. The "Program" section is a broad list of the type of events that are appropriate to that open space. "Typical Facilities" describes the kinds of facilities that would likely be found, consistent with the intent of the particular open space. The list is not exhaustive, however, and other similar uses may be appropriate provided they are consistent with the purpose and program. Both public and private facilities may be provided, which will determine which entity or entities own the open space, and what entity is responsible for maintenance. In some cases, there may be cases where access, ownership or maintenance are shared by multiple entities. The Parks and Open Space Master Plan for River Islands Phase 2 ("Parks Master Plan") adopted by the City provides further details with regard to the purpose and program of the various parks and open space components.

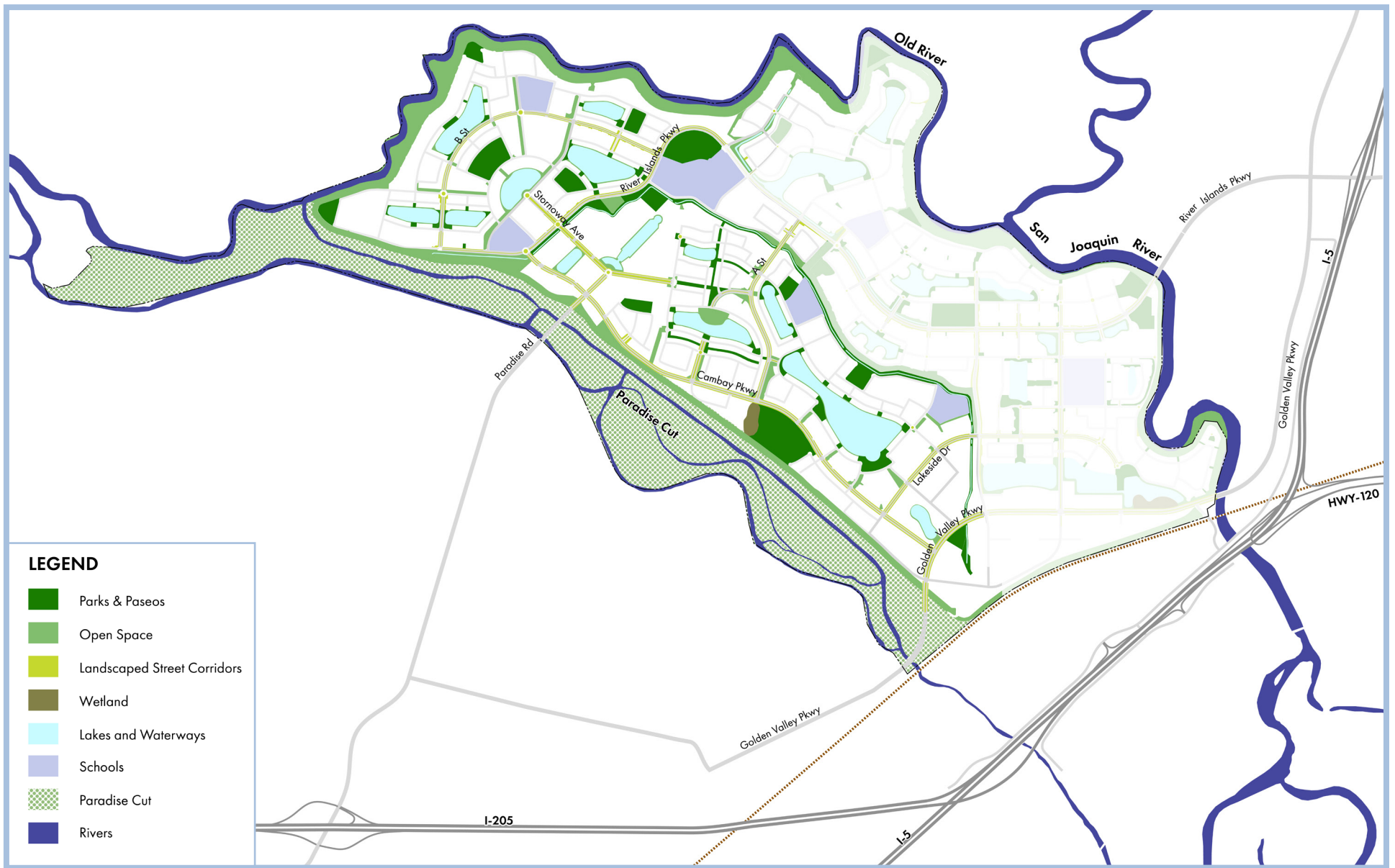


Figure 1.1: Phase 2 of River Islands Open Space System Components

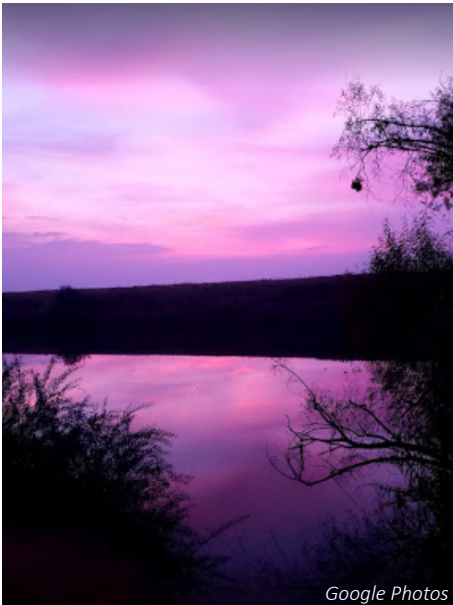


1.1 RIVER SYSTEM INTERFACE

River Islands is surrounded by a series of channels of the San Joaquin River, which provide a continuous buffer and unique open space amenity for the community. Phase 2 of River Islands is bordered by Paradise Cut and the Old River in Particular. The primary goals for this major open space component are to:

- 1) enhance flood control by providing a robust urban levee system and hydraulically connected lake system for stormwater storage;
- 2) protect and enhance existing habitat that can support native plants and animals;
- 3) create a community edge that has a familiar river edge appearance; and
- 4) provide recreation opportunities.

This section describes each of the major channels and their role in the River Islands open space system.



Google Photos



Google Photos



Google Photos

Rivers and Wetlands Character

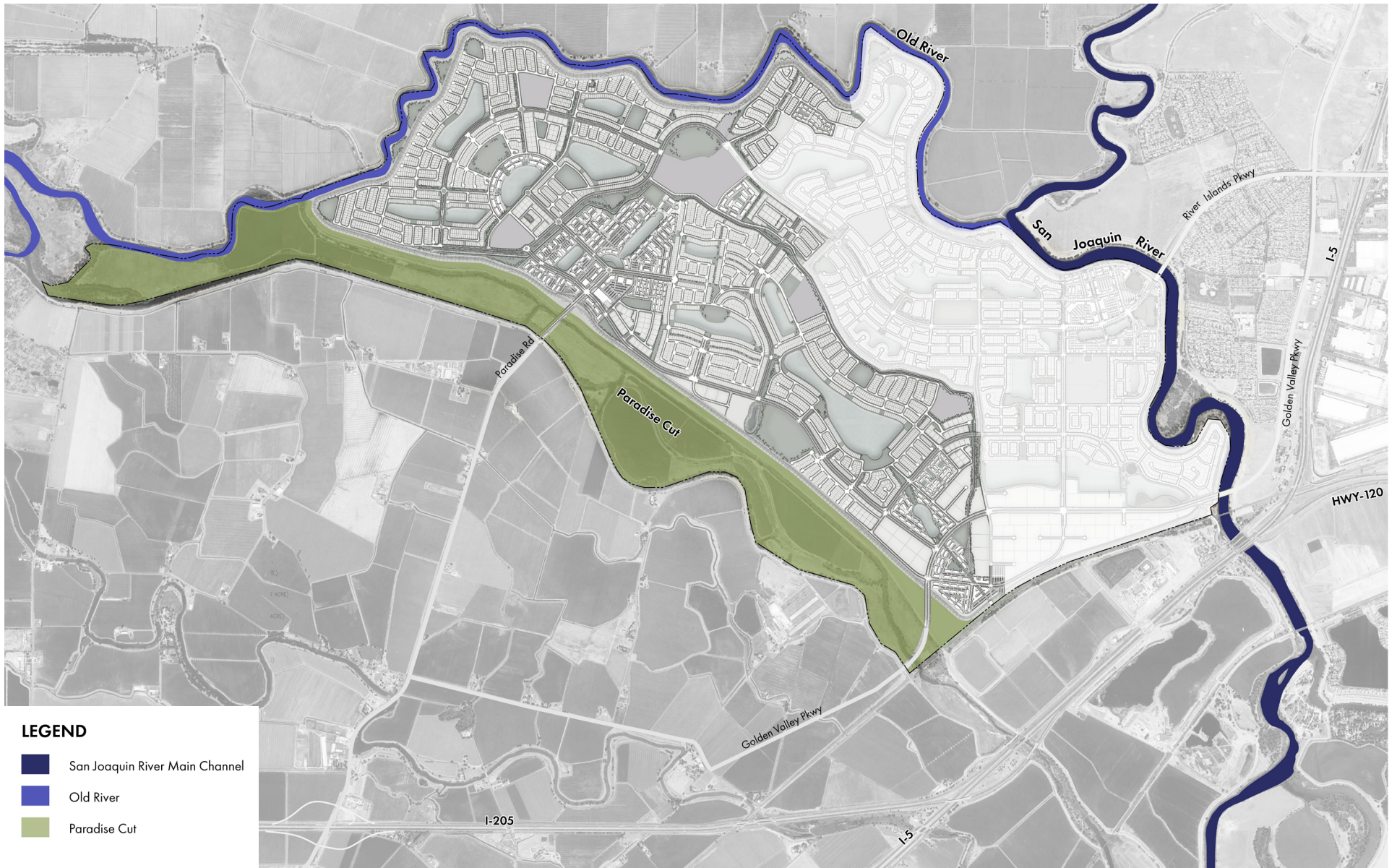
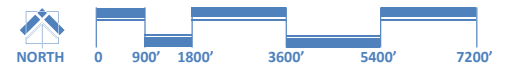
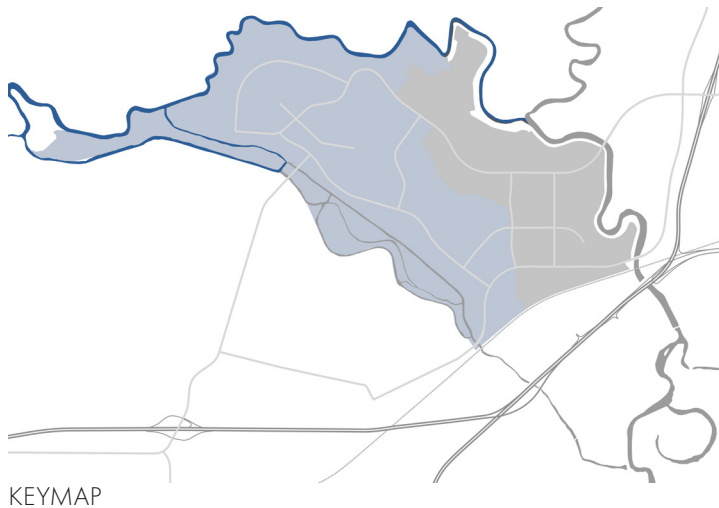


Figure 1.2: Rivers and Wetlands





Old River Channel

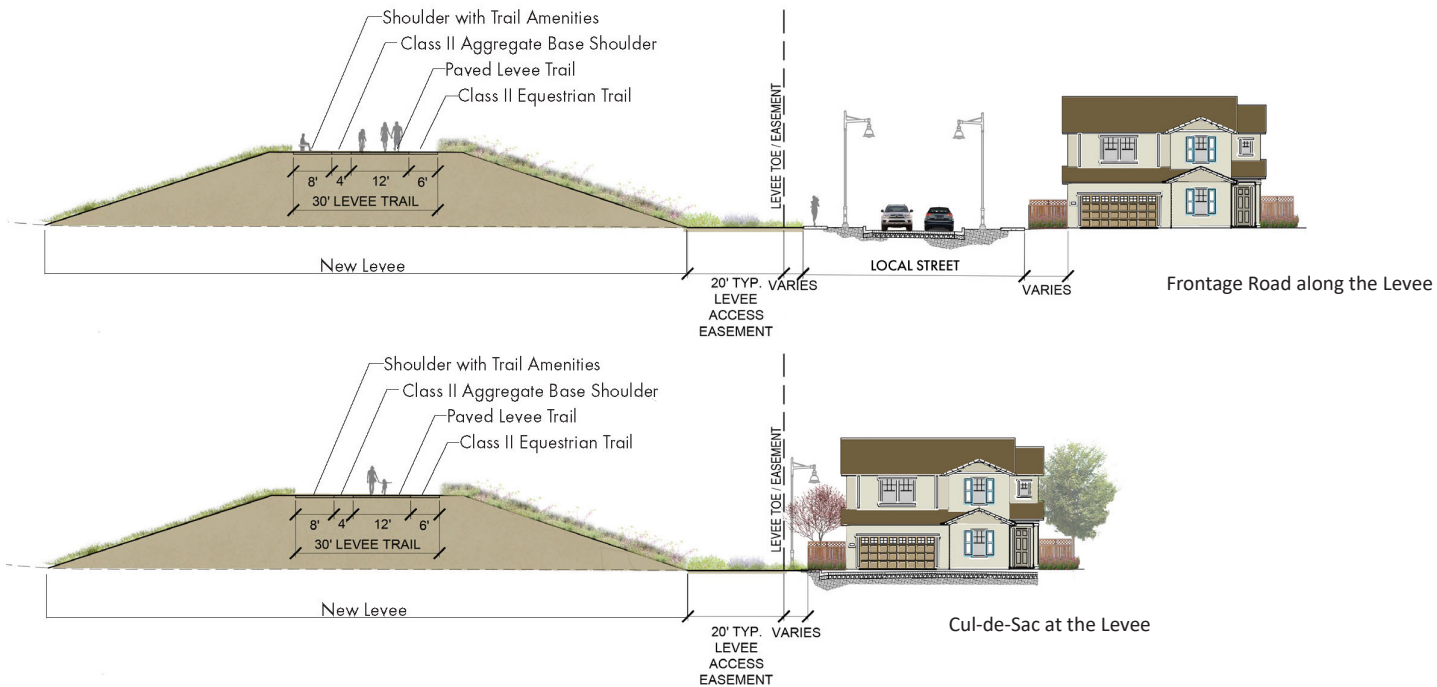
The Old River channel of the San Joaquin River system forms the northern boundary of River Islands. The 200- to 300-foot wide channel is bordered entirely by residential neighborhoods and associated open space. Public access to the river banks and water surface will be limited to areas where designated trails or parks are located adjacent to the river, and will contain a trail system that will be built on the levee crown of the setback levee. Figure 1.3 provides conceptual cross sections for the two typical interface conditions along the levee: 1) a residential frontage road between private lots and the toe of the levee, and 2) private lots terminating at a cul-de-sac at the toe of the levee. A 20-foot levee access easement will be provided at the toe of the levee for both conditions.

Since the Old River levee system actually contains two levee systems currently, (Federal Project levee and RD 2062 urban levee), a proposal in the future to degrade the existing Federal Project levee and place the fill within the two levees will create a monolithic open space area where the RD 2062 urban levee would become the federal levee and the older degraded levee section would be vegetated with natural trees, shrubs and grasses to improve the aesthetics of the river bank. This would provide shaded riverine aquatic habitat areas as well as additional recreational opportunities that cannot be achieved otherwise. It should be noted that this proposal takes federal and state approvals that are not in the scope of this document.

At the head of the Old River and the San Joaquin River, the California Department of Water Resources authorities currently place and remove stone rip-rap in the channel twice a year to prevent salmon from migrating down river in the Old River channel, where they would be destroyed by water pumping facilities of the State Water Project. The State is reviewing proposals for a permanent facility that may include locks for boats and other vessels to transverse the area when river flows from the San Joaquin River are blocked from flowing into the Old River.

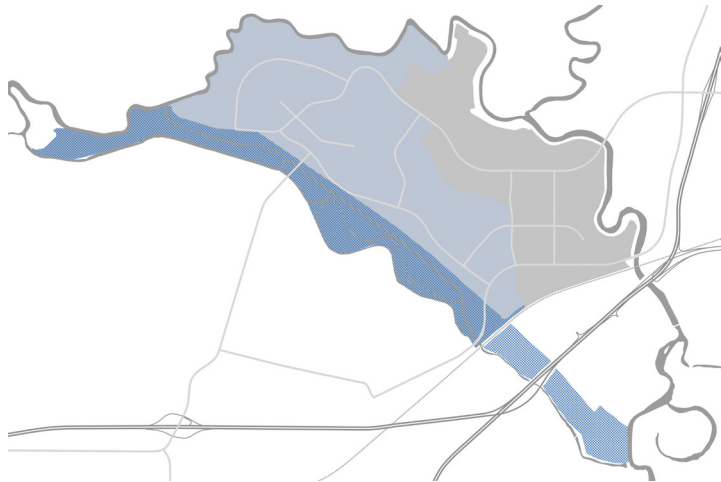


Old River Character



Source: O'Dell Engineering

Figure 1.3: Typical Plan and Sections of Development along the Old River



KEYMAP

Paradise Cut

Paradise Cut is a flood bypass constructed by the U.S. Army Corps of Engineers that connects the San Joaquin River with the Old River. It covers over 900 acres along the southwestern boundary of Phase 2 of River Islands and is nearly 7 miles long. It is more than one-half mile wide at its widest point, and has been historically farmed. Agricultural uses will remain until and unless replaced with eventual eco-restoration oriented open space.

Paradise Cut will continue to serve as a seasonal floodway, agricultural use, and wildlife habitat. The land surface in Paradise Cut is exposed most of the time; only during high river flows in the San Joaquin River will significant areas be under water. The eco-restoration efforts for Paradise Cut will necessitate federal and state entitlements that are expected to be obtained towards the end of Phase 2.

Purpose

- Flood bypass
- Native species habitat
- Passive recreation
- Agriculture
- Education/nature study
- Open space buffer
- Irrigation water source

Control

- Access: public, but limited
- Ownership: public
- Maintenance: public or non-profit

Potential Program

- Wildlife preservation
- Nature observation
- Boating
- Farming

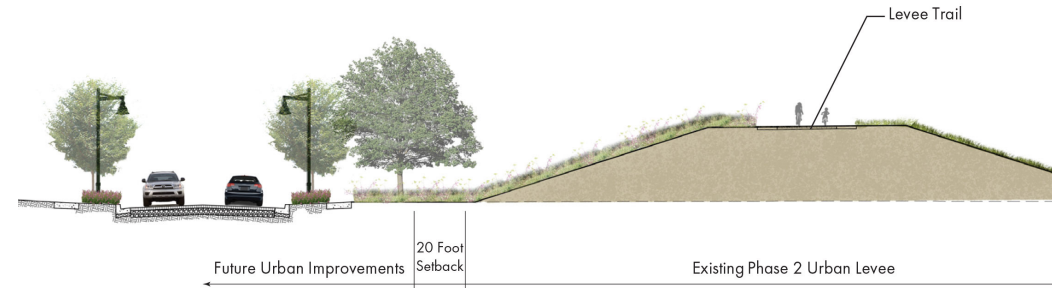
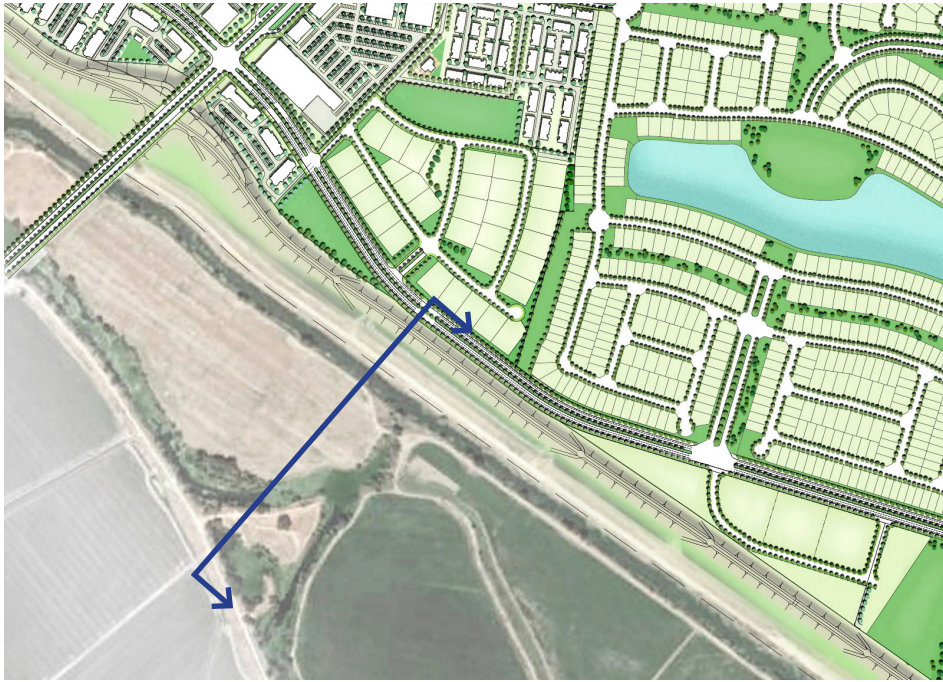
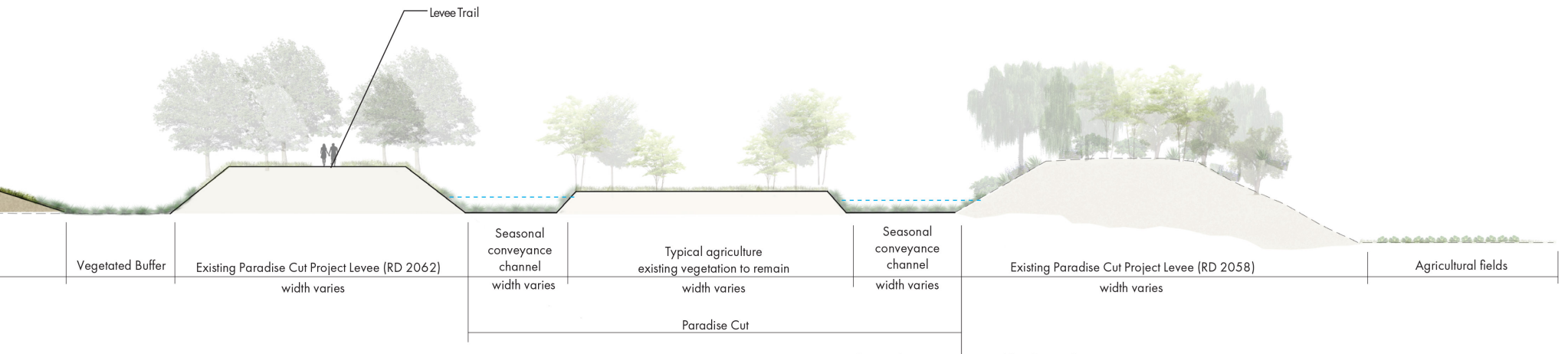


Figure 1.4: Typical Section of Paradise Cut



Paradise Cut Character



Credit: O'Dell Engineering

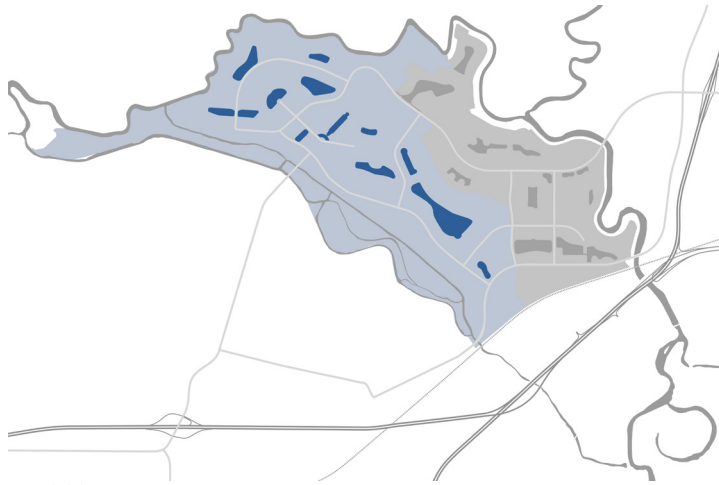
1.2 LAKES & WATERWAYS

Lakes and waterways are the focus of the internal open space system within Phase 2 of River Islands, and are intended to be easily accessible and usable. Although these types of open spaces are primarily intended for passive use, some active uses can be expected on both the lake surface, such as rowing or sailing, as well as in the adjacent park areas. Figure 1.5 identifies the major components of the lakes and waterways in the River Islands Phase 2 open space system.



Lakes and Waterways Character





KEYMAP

Purpose

- Stormwater retention
- Recreation amenity
- Visual amenity

Potential Program

- Boating
- Fishing
- Passive use of lake edges in limited areas

Typical Facilities

- Boat launch
- Fishing pier(s)
- Lake aeration and mechanical facilities, pump station

Control

- Access: public, but limited
- Ownership: public and private
- Maintenance: public

Lakes

The lake system of River Islands provides for functional, recreational, and aesthetic community needs. These lakes will function as storage basins for all stormwater runoff that occurs on River Islands as part of the overall flood protection system. Under extreme storm events, pump stations will discharge stormwater into Paradise Cut. While water surface elevations will vary throughout the year, special engineering features integrated into the design of the lake will keep the water within a 2- to 3-foot range most of the time. The depth of the lakes may vary 15 to 30 feet or more and are connected to groundwater.

As recreational amenities, the lakes will be primarily used for boating and fishing. Fishing will be limited to catch and release. Only hand, wind, or electric-powered boats will be allowed for private individuals. Most private lots with lake frontage will have docks at which a small boat can be stored. In some instances, the docks are shared among multiple lots. Although the quality of the water in the lake will be good, swimming will not be allowed in the lakes.

The edges of the lakes are subject to seasonal inundation and may have a variety of treatments. A majority of the lake edges will have a natural appearance and public access will be limited to certain open space areas.

Lakefront Parks

Lakefront parks are neighborhood or pocket parks adjacent to the lakes. These parks may vary in size from less than one acre to several acres or more. The program for a lakefront park may include passive uses, such as picnicking. A small boat dock/fishing pier and a place to rent boats may also be a feature of these parks. Other possible elements include a wetland area, a boardwalk system, a wildlife viewing area, and an interpretive kiosk.

Lakes may feature lakefront linear parks along the edges in some instances. These linear parks are intended for passive enjoyment of the lake and will include trails for walking and bicycling, as well as seating areas. Small public gathering spaces suitable for picnics or other informal activities may also be provided.



Lake Edge Character - Lake Park



Lake Edge Character - Private Development



Lake Edge - Public Open Space



Lake Edge - Private Development

Figure 1.6: Typical Plan and Section of Development along the Lakes

Credit: O'Dell Engineering

1.3 COMMUNITY PARKS

Community parks are primarily intended to serve the needs of organized sports, community gatherings and active recreation, and will help support the athletic needs of the school system and other community-oriented athletics. A number of these parks, which vary in focus and in facilities, are planned for Phase 2 of River Islands. They will be dispersed throughout the community, and facilities within these parks will be carefully placed to minimize adverse noise, light and traffic impacts on adjacent residential neighborhoods. These parks may also include shared facilities with the school sites.

Figure 1.7 illustrates the proposed community park locations within Phase 2 of River Islands. As with all parks in River Islands, the Parks Master Plan and Neighborhood Development Plans (NDPs) will provide detail on the final sizes, locations and amenities of each community park.

Purpose

- Active recreation for the entire community
- Shared facilities with school (where applicable)

Potential Program

- Active play fields and game courts (lighted and unlighted)
- Passive recreation areas

Control

- Access: public (for shared school park, public access may be controlled during school hours)
- Ownership: public (school district where applicable)
- Maintenance: public

Typical Facilities

- Baseball diamond with backstop
- Soccer/football practice fields
- Hard court play areas
- Picnic facilities
- Restrooms
- Walk trails
- Drainage
- Water-play features such as splash pads
- Amphitheater or performing arts (where applicable)
- Parking



Community Park Character



1.4 NEIGHBORHOOD PARKS, POCKET PARKS, & LINEAR PARKS

Neighborhood parks, pocket parks and linear parks are the heart of the local park system within Phase 2 of River Islands. Together they provide local, easily accessible open space that is within a few minutes walking distance of nearly all homes. Neighborhood parks are generally larger than 4 acres in size, and intended for use primarily by local residents.

Pocket Parks are generally less than an acre in size and provide intimate greenspace within residential neighborhoods. Linear parks are open spaces with trails within residential neighborhoods that provide pedestrian connectivity and direct, convenient access between parks, streets, and other destinations. The form, size, and activities of linear parks will vary according to the character of the neighborhood in which they are located.

Figure 1.8 illustrates the proposed neighborhood park, pocket park, and linear park locations within Phase 2 of River Islands. As with all parks in River Islands, the final size, location and amenities of neighborhood parks, pocket parks, and linear parks will be determined with the Parks Master Plan and NDPs.

Purpose

- Lake physical/visual access (where applicable)

Potential Program

- Play fields (informal)
- Nature observation (where applicable)
- Picnicking (including shaded areas)
- Strolling/jogging

Typical Facilities

- Open lawn
- Hard court play areas
- Picnic facilities
- Seating
- Walk trails
- Children's play area/tot lot
- Drainage

Control

- Access: public
- Ownership: public
- Maintenance: public



Neighborhood Park Character



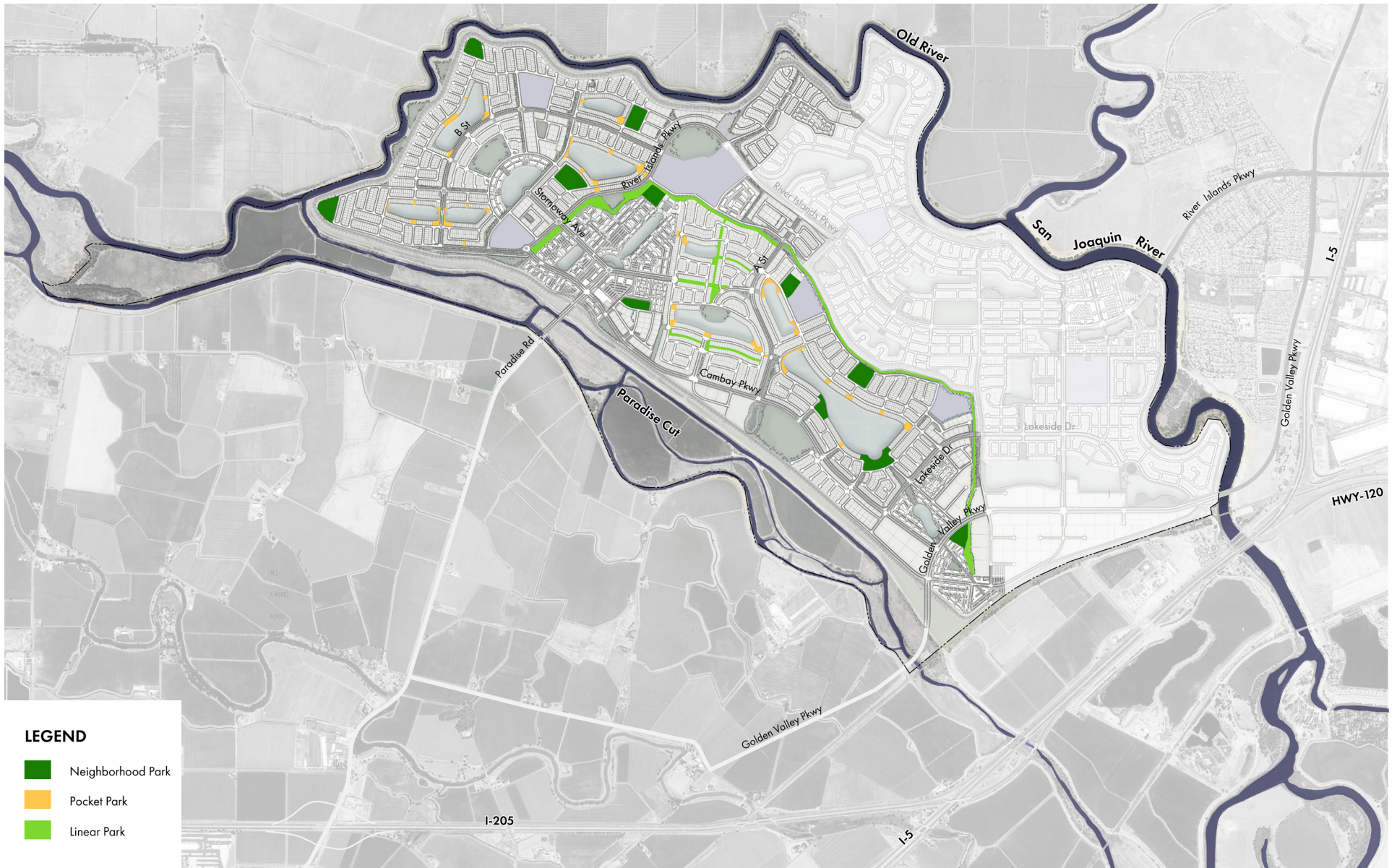


Figure 1.8: Neighborhood Parks, Pocket Parks, and Linear Parks



1.5 TRAILS

The trail system in Phase 2 of River Islands forms an interconnected, hierarchical network that provides access and recreational opportunities throughout the community, encouraging residents to walk or bicycle to work, school, and their neighbors. The trail system utilizes the extensive levee system constructed for flood protection, as well as dedicated rights-of-way generally adjacent to streets to create linear parks. The levee system in Phase 2 of River Islands will interconnect with the Phase 1 trails, including the San Joaquin River Trail that has been dedicated as the Bennie and Joyce Gatto Trail. Pedestrian bridges will be constructed in certain locations to continue the trail system over major arterial streets and other landmarks.

Figure 1.9 illustrates the proposed network of various trails within Phase 2 of River Islands. The Parks Master Plan provides further detail on trail design with regard to access, amenities, signage, planting, and surface materials. Final size, location, and amenities of the trails will be shown in the NDPs.



Trails Character



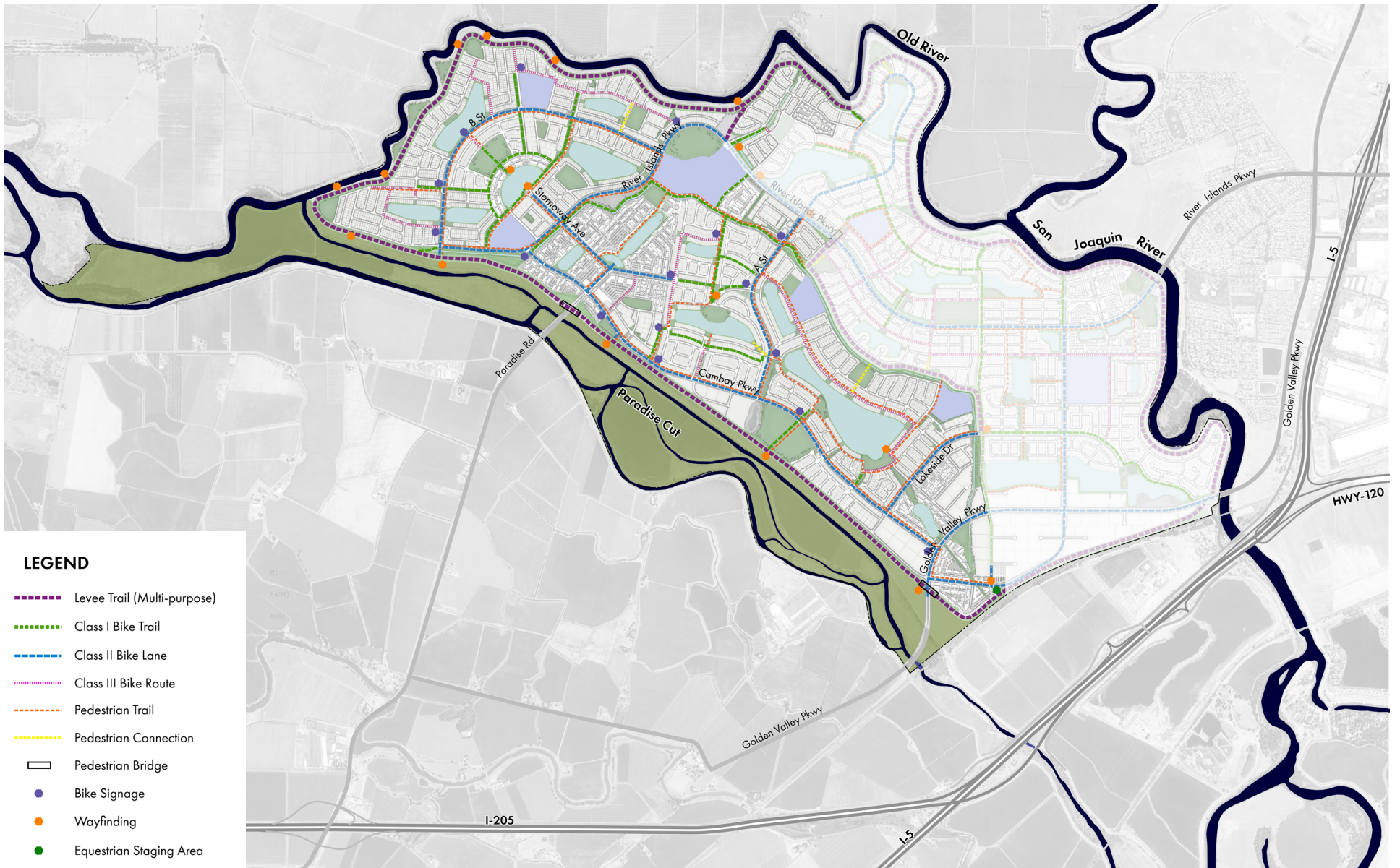
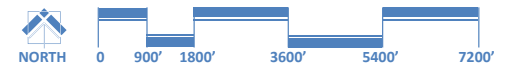


Figure 1.9: Trails



1.6 PRIVATE RECREATION FACILITIES

Adjacent to or within public open spaces, there may be facilities intended for use only by River Islands residents. In some cases, the use of these may be limited to the residents of a particular neighborhood. They may include a community room, spa, tennis court , bocce court or other similar facilities. The facilities will be integrated into the overall design of the park to create a unified appearance.

Private recreational facilities may also be constructed for community-wide purposes, such as Islander's Field in Phase 1 of River Islands, and could be made available for public use under separate agreements.



Private Recreation Character



Islanders Field

DAHLIN



Islanders Field

1.7 LANDSCAPING

Landscape is a key component of River Islands' identity. The landscape of River Islands plays an essential role in creating an environment that is both attractive and meaningful, and provides a lasting presence along the streets, on river and lake edges, and in various parks within the community. Its meaning will come from the way it reflects River Islands' unique context, derived from the site's two major influences: the San Joaquin River Delta and the agriculture fields that permeate the valley.

Landscaping will be further addressed in several different subsequent planning documents. For public parks, recreational opportunities, trails, and plantings will be addressed in the Parks Master Plan. Further detail will be provided in NDPs for each planning district. For individual builders and private development, planting and irrigation details will be provided in the Architectural Design Guidelines and Development Standards (AG/DS) for each planning district.

Plant Palette

The plant palette for the overall parks and open space areas of River Islands will be provided in the Parks Master Plan. The plant palette may be further customized for each district by the individual NDPs and AG/DS documents. Each plant palette section will provide details on requisite street trees, shrubs, ground covers and grasses to encourage diversity in the landscape. In general, trees and shrubs should be non-root evasive, and drought/insect resistant. The details provided will also include a matrix of the allowed species, sizes and planting details. It is intended to assist users in providing the proper plantings within a given development area.

Water Conservation

Water conservation is an important element in the landscape design of River Islands. Plant materials intended for use should be native to the Delta and survive with little supplemental water during summer, once they are established. In addition, a non-potable irrigation water system will be utilized, that will include lake, river and recycled water sources to minimize potable water use for public and private landscapes, as appropriate.



2 ARCHITECTURAL DESIGN PRINCIPLES



Neighborhood Character

CH.2: ARCHITECTURAL DESIGN PRINCIPLES

The character and quality of River Islands' architecture is an essential element in creating the overall identity of the community. There will be a variety of architectural styles throughout Phase 2 of River Islands, reflecting both traditional and more modern, progressive styles. The Architectural Design Guidelines and Development Standards (AG/DS) document for each district will provide the details for acceptable architectural designs, setbacks, lot coverage, materials, colors and similar items. The intent of each individual AG/DS document is to provide the STDRC and the City of Lathrop Community Development Department with the tools to review and evaluate proposed new homes for each River Islands development district. The following design principles are generally provided to guide future AG/DS documents.

2.1 REGIONAL PRECEDENTS

The San Joaquin and Sacramento River Deltas are rich in precedents that can serve as the basis for the architecture of River Islands. These not only include the historical architectural tradition, but the climate, natural environment, and cultural history.

River Edge

The use of the river system for commerce has been largely replaced by recreational use. Still, remnants of the rivers' use as the primary conduit for goods and materials, as well as the agricultural products of the region, remain. The simple, economical forms and materials of river edge industrial buildings, and their docks and quays provide a meaningful source of inspiration for the architecture of buildings in the Paradise Cut Village Center and Employment Center/ Transit Oriented Development.

Similarly, the tradition of placing large homes on high ground near the river edge, with their small docks and boat houses provides inspiration for the design of residential buildings along the water edges.

River Delta

Because of the abundant water, flat slopes and easily worked soils, river deltas have always been desirable for agriculture. The tradition of farmsteads in the Delta, with their simple farmhouses and outbuildings, can be a particularly rich source of inspiration for residential building design.

Mediterranean Climate

The climate of the Delta is Mediterranean: cool, moist winters are followed by warm, dry summers. The prevailing westerly winds bring in cool air from the Pacific, making evenings pleasant, even in the summer. Shade provided by roof overhangs, carefully placed large trees or shade structures extend the usable space of buildings into the landscape and add to the richness of the environment and the ways residents can relate to their environment.

Delta History & Culture

Originally part of the Rancho Pescadero, River Islands has a direct historical connection to the Spanish and Mexican Land Grant system that pervaded California in the eighteenth and early nineteenth century. The discovery of gold north and east of River Islands, and the subsequent

boom are also part of the region's architectural influences. The simple, slapdash architecture of boom towns, and the subsequent opulence of the Victorian Era figure in the architectural history of the region. The traditions of agriculture and river-based industry are the most prevalent historical influences, however, and provide perhaps the most fitting stimulus for architectural design at River Islands.

2.2 THREE ARCHITECTURAL DISTRICTS

River Islands is divided into three architectural districts based primarily on the dominant use within the district: Residential, Village Center and Employment Center/Transit Oriented Development. The requirements for the architecture of each district vary, as described in the following paragraphs.

Residential

The architecture of the residential areas in Phase 2 of River Islands will contain a wide variety of architectural styles and influences. Generally, the styles may be based on historical precedents from the region, such as those found in farmsteads, along the river edge, or in the older residential neighborhoods of cities like Sacramento or Stockton. Modern adaptations of these styles may also be developed, though the intent is to create an appearance and feeling of traditional neighborhood streets and neighborhoods. The mixing of styles within individual neighborhoods will be limited so that visual unity can be achieved, and strange juxtapositions avoided.

Village Center

The architecture of the Paradise Cut Village Center will vary according to land use and location. Retail and commercial architecture could be inspired by historical commercial and industrial building types that might be found in other riverfront towns. Ideally, this area should appear as if it developed over time, with the participation of numerous designers. Unity will be achieved primarily by uniformity of use and development standards, such as building mass and setbacks. A NDP and AG/DS, or equivalent Paradise Cut Village Center Plan will detail the specifics of these standards and the architecture to be utilized.

Residential areas of the Village Center may follow the guidelines for the Residential district described above, or may show other modern influences. Individual development proposals will be judged by the STDRC on the effectiveness with which they create an architectural richness in the Village Center, based on the NDP and AG/DS, or equivalent Paradise Cut Village Center Plan.

Employment Center/Transit Oriented Development

Employment Center/Transit Oriented Development will be a mixed-use area likely to be implemented with more contemporary styles of architecture for its higher density residential housing. For office, retail and commercial uses, the architectural styles will be the most modern of any of the three districts. Larger floorplate office and commercial buildings will generally be simple, modern, rectilinear forms with flat roofs. Masonry, concrete tilt-up and other economical building types will be predominant. Buildings with historical references will likely be limited to retail centers or restaurants. A NDP and AG/DS, or equivalent Employment Center/Transit Oriented Development Plan will detail the specifics of these standards and the architecture to be utilized.

2.3 CONSISTENT FORMS & MATERIALS

Within each architectural district, there should be consistency in several important characteristics, such as roof design, materials, elevation treatments and color.

Roof Design

Similarity of roof pitch, color and materials is especially important in the Residential district, and particularly within an individual neighborhood. More variety in roof styles will be tolerated within the Paradise Cut Village Center and Employment Center/Transit Oriented Development.

Shingle roofs will be predominant in residential neighborhoods, though tile may also be used in some areas. Metal roofs consistent with the industrial tradition of riverfront buildings will be permitted in the Village Center and on retail or restaurant buildings in the Employment Center/Transit Oriented Development.

Materials

The dominant materials of Phase 2 of River Islands will be wood, masonry, brick, stucco, and metal. Within the Residential and Village Center districts, it is likely that all these materials will be used, though the juxtaposition of contrasting materials in residential areas will be limited. Brick and masonry are likely to dominate office buildings in the Employment Center/Transit Oriented Development, though wood may also be used.

Reflective glass curtain walls will not be permitted in Phase 2 of River Islands, though non-reflective glass may be used in the Employment Center/Transit Oriented Development. Metal-clad buildings may be used in the Village Center or in the Employment Center/Transit Oriented Development, consistent with the application of the historical tradition of riverfront architecture.

Elevation Treatments

Building elevation treatments will be most consistent within the Residential and Employment Center/Transit Oriented Development districts. Within individual neighborhoods, there will be a consistency in window and door size location and treatment that helps create a pleasing visual unity, though the styles may vary considerably. Traditional precedents will dominate in the Residential district, while modern precedents will be common in the Employment Center/Transit Oriented Development. The Village Center will have the most variety, consistent with its more diverse visual appearance and its role in the community.

Color

The guidelines for the application of color to buildings in Phase 2 of River Islands is similar to the guidelines for materials and elevations. Within the Residential district as a whole, there may be a great variety of colors, though individual neighborhoods will display a limited range. Juxtaposition of colors will be permitted in the Village Center, and may also occur in the Employment Center/Transit Oriented Development, consistent with other architectural requirements for these districts.

2.4 ENERGY EFFICIENCY

All buildings within Phase 2 of River Islands shall be designed to conserve energy and to meet current state Title 24 standards as amended. Among the methods that should be considered are (and may be mandatory by state law):

- Passive solar design, such as thermal masses to absorb winter sun energy, and roof overhangs and carefully placed deciduous trees to provide summer shade;
- Active solar design, such as solar collectors to heat water, or photo voltaic cells to generate electricity;
- Energy efficient mechanical equipment for heating and cooling, such as heat pumps;
- Extra thermal insulation in roofs and walls to control heat gain and loss;
- Operable windows in commercial buildings;
- Home integrated systems, wireless PC based systems that allow homeowners to program appliances to restrict usage during peak energy periods;
- Load shifting technologies, such as thermal energy storage for residential and commercial use that moves the operation of air conditioning compressors from on-peak operation to off-peak hours;
- Thermal rated glazing, including reflective coatings to reduce heat load in the summer;
- Utilization of Energy Star rated appliances;
- District heating and cooling, where feasible and economical, to medium- and high-density residential areas, the town center and employment center areas;
- Distributed generation facilities, including fuel cells, wind technology, photovoltaics, provided such facilities are consistent with other requirements of the UDC, Specific Plan, and other regulations; and,
- Geothermal heat pumps used to heat and cool multiple homes in an area where such facilities are feasible and economical. Use of the lakes for such facilities is permitted within River Islands. Use of water from the San Joaquin River system may be subject to additional environmental review, but is permitted within River Islands, subject to the UDC, Specific Plan and other regulations.



RIVER ISLANDS

SPEED
LIMIT
25

NO
PARKING
ANY
TIME

03

COMMUNITY IDENTITY
ELEMENTS



Sources of inspiration for design vocabulary of identity elements

CH.3: COMMUNITY IDENTITY ELEMENTS

This section describes how a variety of disparate but interrelated elements will help create and reinforce River Islands' distinct identity for the Phase 2 areas. Generally, these elements are physical objects within the public rights-of-way or on public land that are pervasive and visible, such as light fixtures, walls, or bridges. Coordinating the design of these objects by establishing simple guidelines as to their form, materials, and color provides visual unity and creates identity.

Like open space, landscape, and architecture, the design vocabulary for River Islands' identity elements is based predominantly on visual ingredients of the existing Delta landscape. These include the following:

- The long, straight lines of row crops, orchards and vineyards;
- the colors green, blue and brown that represent the dominant colors of the landscape: green of the agricultural fields and natural delta vegetation, blue of the sky and river, and brown of the rich Delta soils;
- the steel of the trusses in the old San Joaquin River bridges and the straightforward agricultural buildings that dot the landscape; and
- the simple concrete forms of major Delta infrastructure, such as the irrigation canals.

These four sources provide the design vocabulary for the River Islands identity elements described in this section.

3.1 BRIDGES & CAUSEWAYS

Bridge & Causeway Concept

Phase 2 of River Islands will include construction of three bridges that connect the community with the rest of Lathrop and the region. Figure 3.1 shows the bridge and causeway locations for Phase 2. The following criteria apply to these infrastructure elements. The design of these bridges should be consistent with the following guidelines. The design and construction of these bridges will be led by the City, in consultation with the STDRC.

Infrastructure Element Hierarchy

The treatment of infrastructure elements will vary according to their importance to the overall identity of the community. The main elements of the infrastructure are described in this section, which are arranged in the order of importance to the overall identity of the community.

Simple, Modern Forms

The form of the bridges and causeways should be simple and modern. Bridges may also incorporate standard box beam construction in hunched or straight line forms, and arches may be used in bridge elements that cross the lakes.

Masonry and Metal

Materials for the bridges may include masonry and/or metal elements, consistent with durability, strength and maintenance requirements.

North Golden Valley Parkway Bridge

This bridge over the San Joaquin River will replace the existing Manthey Bridge as part of a bridge replacement project managed by the City, connecting the Employment Center in River Islands to Mossdale Village and other areas of Lathrop via Golden Valley Parkway.

River Islands will coordinate with the City on the location of the bridge to correspond with the alignment of Golden Valley Parkway into the Employment Center District. While this bridge is within the Phase 1 area, the first phase of its construction will likely occur during the initial stages of Phase 2 development.

South Golden Valley Parkway Bridge

The southern end of Golden Valley Parkway crosses Paradise Cut, potentially as a causeway and connects to Interstate 205 in the future via the Paradise/Chrisman interchange, several miles to the southwest. This bridge will only be necessary should Golden Valley Parkway continue off-site of River Islands as a regional roadway, programmed by the San Joaquin Council of Governments, San Joaquin County, or as part of a multi-agency effort.

Paradise Road Bridge

An additional set of bridges over the Paradise Cut and Tom Paine Slough might be required during the development of Phase 2 to supplement the existing set of bridges and increase regional roadway capacity. Paradise Road is an important regional roadway that connects the project to Interstate 205 via the planned Paradise/Chrisman interchange and the existing MacArthur Boulevard interchange near Tracy. Specific segments of Paradise Road may need to be widened to four lanes to accommodate planned project related traffic.

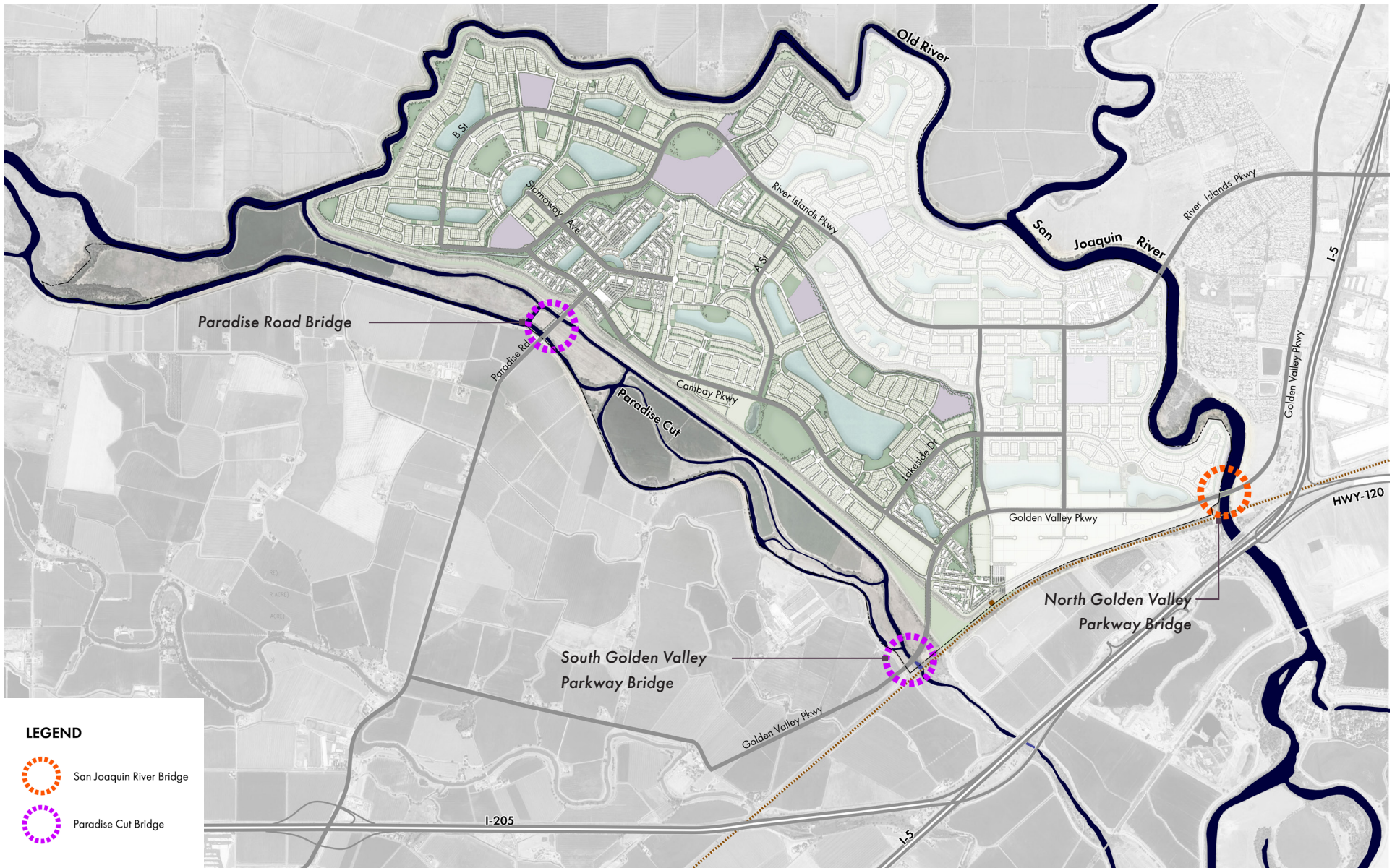
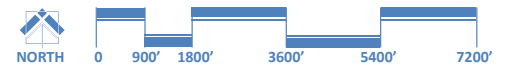


Figure 3.1: Bridge Locations



3.2 COMMUNITY SIGNS & GATEWAYS

Gateway Concept

Gateways provide a means to identify River Islands as a community distinct from others in the region. Occurring primarily on roads at the entrance to a particular district or neighborhood, gateways in Phase 2 will consist of special landscape planting, monuments, signs, and other elements that have a scale appropriate to the size of the street and the place that they identify. The following is a description of the primary design criteria for these features.

Understatement

Gateways should be simple and understated. Trees and other landscaping should complement the dominant element, as they are well-suited to creating an atmosphere appropriate to River Islands. Additionally, the number of different types of materials should be kept to a minimum.

Durable and Long-lived

As permanent landscape features with a life of 30 to 50 years or longer, it is especially important that forms be simple and have a classic appeal. Materials for signs or other gateway monuments should be steel, masonry, or concrete, and should be durable and maintenance-free. Text should be limited to one type face and font consistent with the Phase 1 program, and should be classic in appearance.

Express Community Hierarchy

Gateways should express a hierarchy that reflects the structure of the community, with the community gateways being the most pronounced, the Paradise Cut Village Center and Employment Center/Transit Oriented Development district gateways in the second tier, and neighborhood entries being the least prominent. All should share common materials, forms and styles.

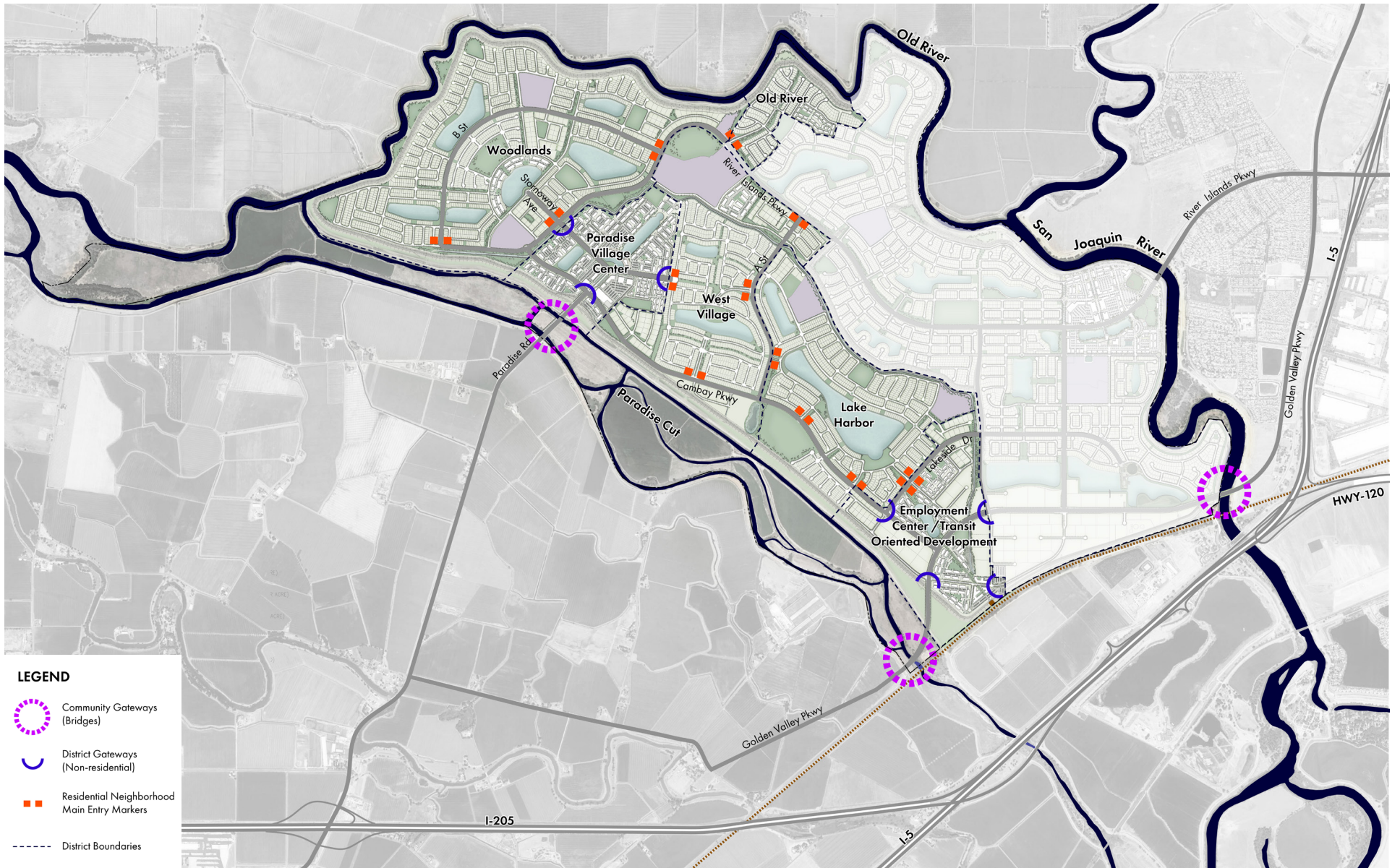
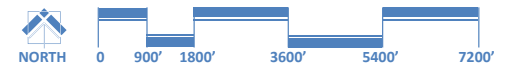
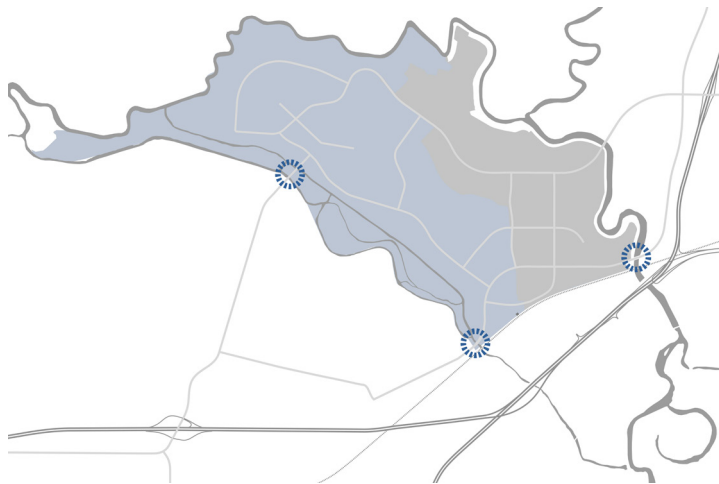


Figure 3.2: Gateways and Entryways





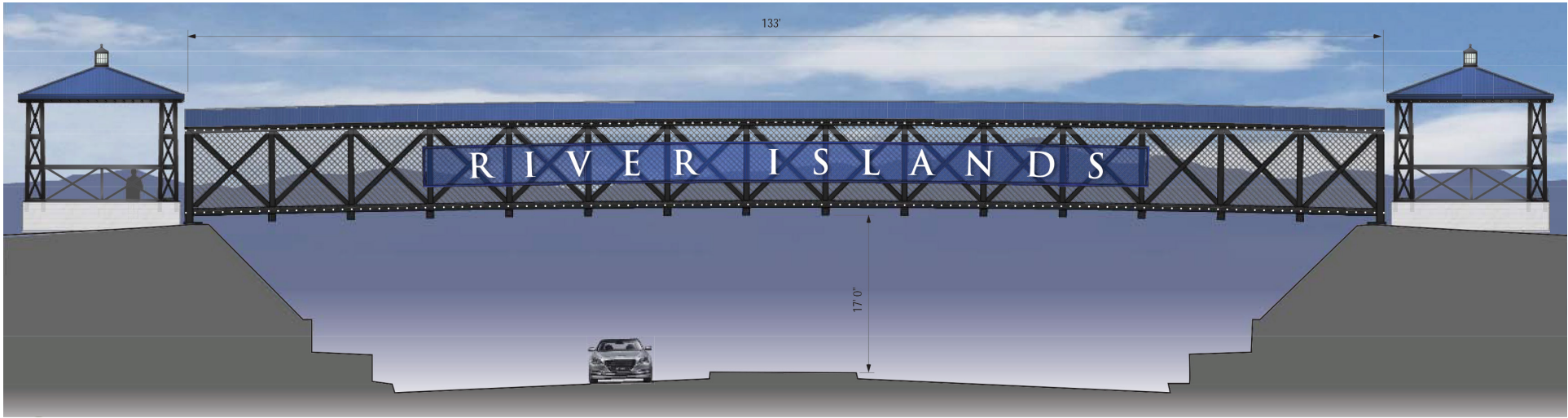
KEYMAP

Community Gateways

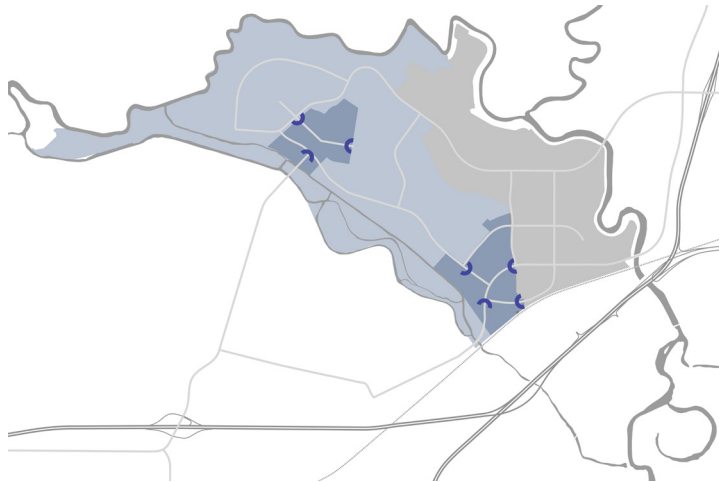
The most important, largest and most elaborate gateways will occur at the three main entries into Phase 2 of River Islands. All are located at points where bridges cross the San Joaquin River or Paradise Cut.

Community gateways are formed by the bridges themselves and the associated signage/lighting elements that cross the rivers at the edge of River Islands on Paradise Road and Golden Valley Parkway. Pedestrian/bike bridges which cross over these major arterial streets at the community entries will also become part of the community gateway feature.

Because of the variety of conditions at each of the three entries, the community gateways will vary in treatments. There will, however, be elements common to all to retain continuity and unity. The design of the bridges will be deliberately incorporated into the overall entry design sequence.



Community Gateway Examples



KEYMAP

District Gateways

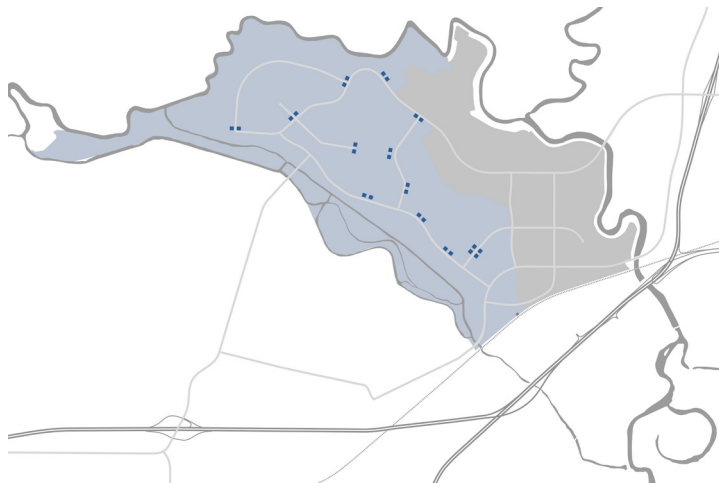
District gateways may occur within or at the edge of districts, and should be less significant than community gateways. District gateways may be closer to markers than true gateway elements, especially if they are located inside a district, rather than at its edge.

Paradise Cut Village Center District Gateways

Village Center district gateways, identity markers and signage should create a welcoming entry into the mixed-use district. Details on the exact placement and style of these gateways will be provided in the future NDP or equivalent Village Center Plan.

Employment Center/Transit Oriented Development District Gateways, Identity Markers, Tenant Signage

Employment Center/Transit Oriented Development district gateways, identity markers and signage may vary depending on the type of uses. A future NDP or equivalent Employment Center/Transit Oriented Development Plan will provide details on these items prior to the establishment of the first office or commercial use in this district.



KEYMAP

Residential District Entries and Identity Markers

Each district NDP will provide details on residential district entries and identity markers. In general, each residential district should have a distinct character, consistent with the rural agrarian context and aesthetics of the Delta Valley that River Islands is part of.



District Gateway Examples for the Village Center, Employment Center/Transit Oriented Development



Residential Entry Examples



3.3 WALLS & FENCES

Walls & Fences Concept

Due to their presence adjacent to arterial and major collector roads, walls and fences are significant visual elements in creating the identity of Phase 2 of River Islands. Generally, the design intent for walls and fences is to make them recede so that they do not visually dominate the landscape. Although other provisions of relevant design documents will help reduce their visual prominence, there is the need for more robust structures in places where sound attenuation is required.

Various wall and fence structures will be used that are visible from the public realm, depending on their location and purpose. In general, the following criteria will be followed:

- Masonry walls will be used where sound attenuation is needed along major arterial streets. Adequate landscaping treatments should be provided between the wall and the sidewalk to prevent these walls from visually dominating the streetscape. Use of pilasters at regular intervals and wall cap is recommended to create a visual rhythm and break up the mass.

- Wood fencing will be used along major collector streets to provide privacy for the individual backyards. Masonry columns may be used in regular intervals to add visual interest and consistency with the arterial streets. Lattice detail at the top of the fence is encouraged. Adequate landscaping should be provided between the fence and the sidewalk.
- Open view metal fencing will be used for rear yards along major open space corridors or around lakes, to provide expansive views to these features and a sense of connection between the public and private realms. Masonry columns may be used in regular intervals.
- Low railing will be used between public roadways and lakes or drainage corridors for safety, while maximizing views to these features.

As with other design elements discussed in this UDC, individual NDPs will provide more detail on the placement and type of public wall and fence structures in relationship to the right-of-way and property line, and individual AG/DS documents on private development.



Example of Wood Fencing and Buffer Landscaping along a Collector Street



Road Railing Example along the Water Edge



Example of Masonry Wall with Pilaster for Sound Attenuation



Open View Metal Fencing Example along Lakes and Open Space

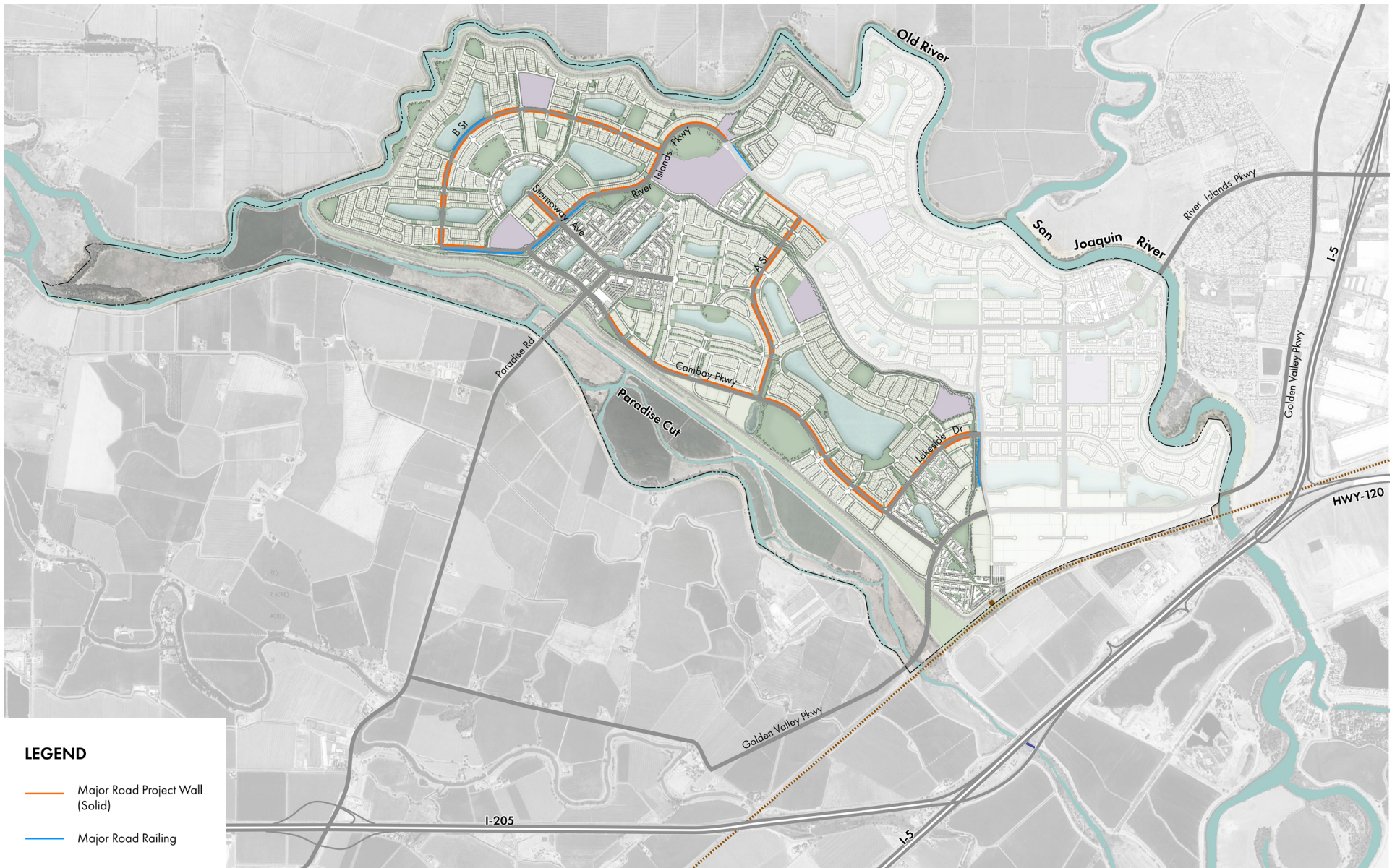
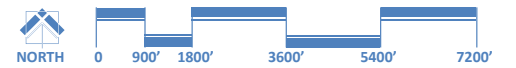


Figure 3.3: Walls and Fences



3.4 STREET FURNISHING

Street Furniture Concept

Street furniture consists of streetscape elements such as bus shelters, seating, trash receptacles, bollards, newspaper racks and kiosks. Generally, these elements are either located in the public rights-of-way, or within public parks and open spaces. Consistency in design for these elements help visually unify a community, district or neighborhood. Street furniture design complements the design of other landscape elements, including light fixtures, walls, fences and signs.

Visually Compatible Family of Elements

With the exception of the Paradise Cut Village Center, street furniture within a particular district will be from the same family. They will share style, form, color and materials to help provide visual unity. Generally, all furniture will be simple and modern in appearance, rather than derived from a particular historical period. Although furniture is intended to provide visual unity, it is not intended to visually dominate the landscape.

Durable, Easy to Maintain

Generally, street furniture will be constructed of powder-coated steel, glass, concrete, or masonry materials, the exception being furniture used in natural areas where wood will be encouraged. The intent is to have furniture that is long lasting and inexpensive to maintain.

Design Details

As with other design elements discussed in this UDC, individual NDPs will provide more detail and guidance on public street furnishings.



Street Furniture Examples

3.5 LIGHTING

Lighting helps communicate a myriad of information about the community, such as areas of night time activity, street hierarchy, or wayfinding. In River Islands, lighting will help provide a consistent character that contributes to the overall identity of the community, and also create a sense of safety and security for the residents. The following principles describe how lighting will help achieve this. As with other design elements discussed in this UDC, individual NDPs will provide details on public light fixtures and individual AG/DS documents on private development regarding lighting.

Identity

Lighting should help create a distinct community identity through consistent, hierarchical illumination of streets, water, bridges, plazas, artwork, landscape, graphics and building exteriors.

Cohesion

Lighting should unite the various community districts and components, and create unique light qualities to define subdistricts for the various activity areas.

Architecture

Lighting should enhance and feature the distinct architectural components within each district, street, park and plaza to create landmarks.

Wayfinding

Lighting should help define the entrances to the community, as well as major streets and intersections, civic, retail, and entertainment tenant buildings, and trails.

Durability

Lighting fixtures should be attractive and durable to help create a sense of quality, dignity and integrity that is important to public perception of the community.

Efficiency

Lighting should be generated by efficient light sources to save energy and minimize operating costs.

Environment

Lighting should be generated from environmentally friendly solutions which limit light pollution, or the disposal of harmful waste products.

Security

Lighting should create a sense of safety and security throughout the community with clear identification of circulation, gathering spaces, and parking facilities.

Visibility

The community entrances and the mixed-use destinations should be prominent at night to attract and welcome the public, with the objective of maximizing and enhancing the user's experience.

Celebration

Important community focus areas such as the Paradise Cut Village Center and Transit Oriented Development, as well as community parks should express their importance with exciting and interesting lighting treatments. Important public gatherings may have special lighting effects.

Domestic

The light intensity, character, and quality of lighting in residential areas should have a low-key, residential character. Lighting of local residential streets, neighborhood parks, and residential buildings should convey a relaxed feeling, free from distracting displays.

Sports Field Lighting Guidelines

The proposed high school and adjacent community park in Phase 2 of River Islands will include illuminated sports fields. All fixtures should be shielded to contain the light within the school and park area boundary, and avoid light spill and glare to the adjacent residential neighborhoods and natural open spaces. The lighting design for sports fields should follow the guidelines below:

- LED based lighting systems are the preferred lighting source.
- Lighting fixtures shall be mounted, aimed, and fully shielded so that their beams fall within the primary playing/activity area and immediate surroundings to prohibit light trespass onto adjacent properties.
- The number and height of the fixtures should be considered in conjunction with the aiming angles, as well as the use of reflectors, visors, and/or shields, to avoid light spill and glare.
- Establish specific time restrictions combined with automated controls.
- Consider dimming systems to control the intensity of light depending on the event type.



Arterial Street Light Examples



Collector/Residential Street Light Examples



Example of Neighborhood Park Lighting



Bridge/Community Gateway Lighting Example



Example of Neighborhood Entry/Signage Lighting

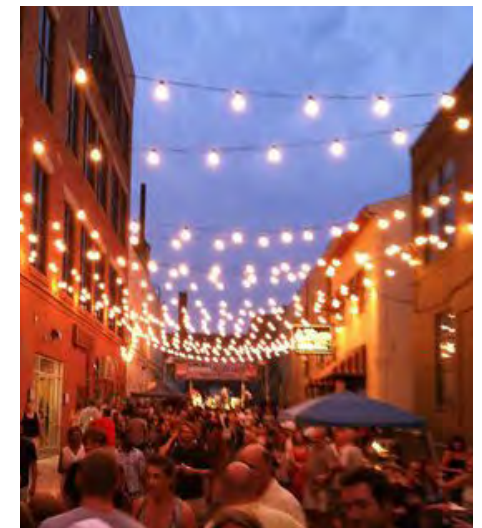


Example of Lighting to Enhance and Complement Significant Community Building Architecture

riverislands.com



Example of Bollard/Path Lighting



Example of Lighting for Community Gathering and Celebration

3.6 BOAT DOCKS

Boat Dock Concept

Boat docks will be found throughout the River Islands community, primarily on internal lakes. To minimize conflicts between dock structures, private boat docks will likely be ganged or shared between individual lots to provide each home with boat dock access but to not over concentrate the lake edge. Public boat docks may also be constructed by the local reclamation district and/or the master developer within lakes for general public use. If federal and state approvals can be obtained in the future, boat docks may be allowed on the San Joaquin River, Old River, and Paradise Cut. If permitted, such docks would be subject to future design guidelines.

Within lakes, boat ramps and/or docks will also be provided for emergency services providers separate from private and public docks. These facilities may also provide access to the public for fishing and other recreational opportunities if allowed by applicable public agencies.



Lake Dock Examples

3.7 STORM DRAINAGE & STORMWATER MANAGEMENT

Drainage & Stormwater Management Concept

Specific design details will be provided in the Phase 2 Master Storm Drainage Plan (prepared by PACE), as well as lake management and operation plans drafted by the local reclamation district for drainage systems associated with the public lakes.

A Visible System

The design and layout of the open space and circulation systems for Phase 2 of River Islands will be configured to allow a portion of the stormwater drainage system to flow on the surface and percolate naturally into the ground water as a best management practice for treatment. Parks (including linear parks), arterial parkways and medians will accommodate this surface flow, and the landscape of these elements will reflect their function in the stormwater management system.

Lakes Integrated with Community Storm Drainage System

The above ground portion of the storm drainage system will be complemented by an underground system of pipes. These two systems will be interconnected to provide redundancy, while permitting rainfall to be directed to swales and other open spaces where water will percolate into the soil along certain arterial street rights-of-way before entering storm drain pipe inlets and directed to the lake system.

Variable Forms

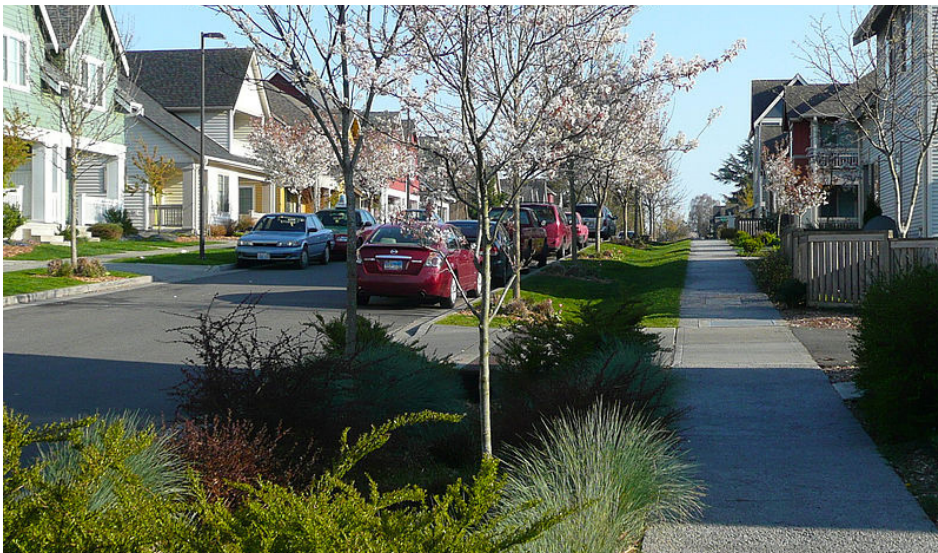
The form and landscape treatment of swales will vary from long, linear swales adjacent to arterial roads or within linear parks, to areas adjacent to the lakes that appear to be natural wetlands.



Lake Wetlands



Rain Gardens



Roadside Swales



Bioswales in Parking Lots

Various Forms of Stormwater Best Management Practices



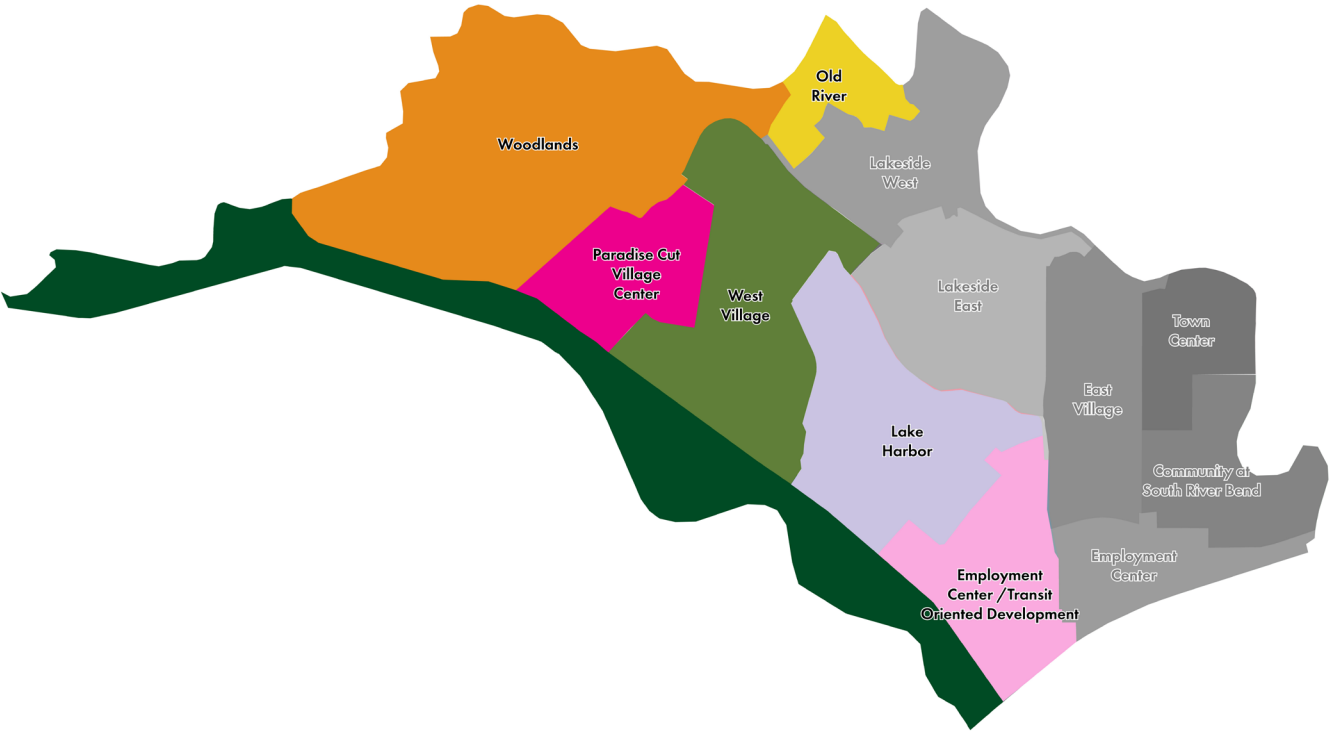
04

**DISTRICT
URBAN DESIGN
FRAMEWORK**

CH.4: DISTRICT URBAN DESIGN FRAMEWORK

This chapter contains high-level, conceptual guidance to be used in subsequent planning applications for each of the six (6) Phase 2 districts as established in the WLSP. Districts are unified geographic areas of River Islands that share common features, such as primary land use and development character. The intent of the District Urban Design Framework chapter is to describe factors that establish the unique character of each district.

The description of each district includes a District Overview, which contains a brief written and graphic introduction to the district. The district illustrative plan that accompanies the written description is intended to show how the district might be developed in a manner that is consistent with the standards and guidelines of the WLSP, and provide guidance to the subsequent NDP and AG/DS documents. However, the plan only represents one solution; other solutions are possible that would be consistent with the WLSP and UDC. An important function of the STDRC will be to evaluate design proposals against the provisions of the WLSP, UDC, NDP and AG/DS documents as applicable.



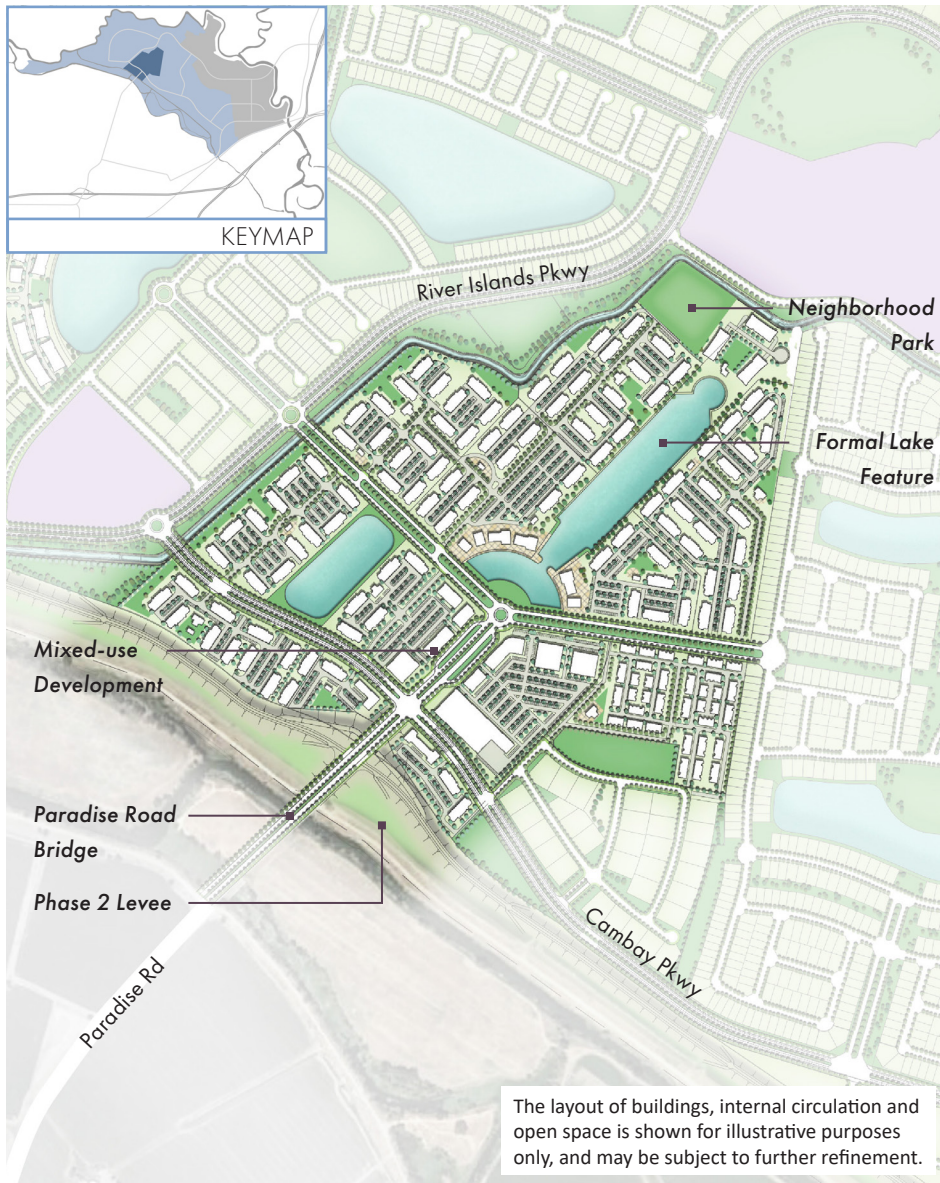


Figure 4.1: Paradise Cut Village Center Illustrative Plan

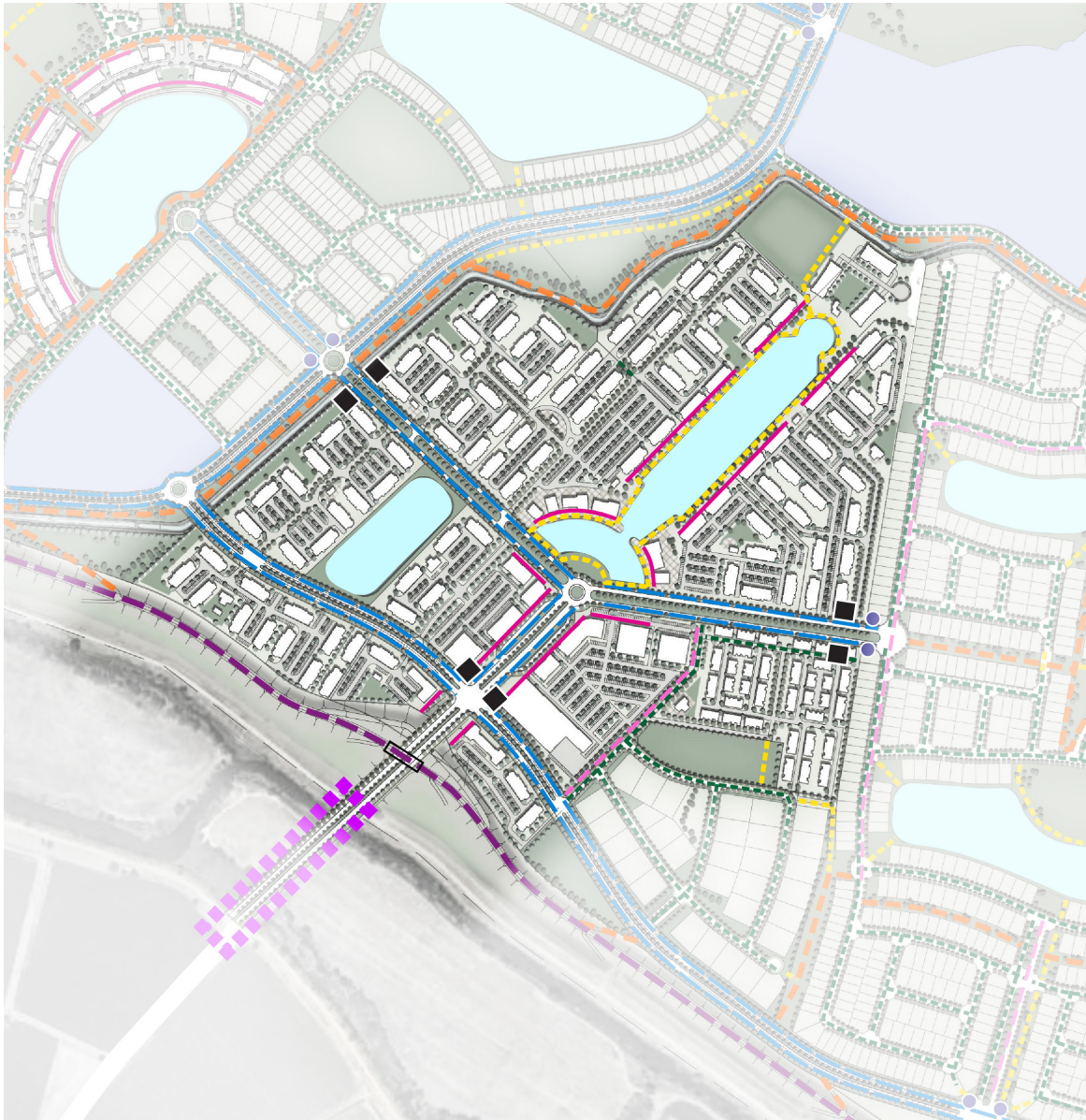
4.1: PARADISE CUT VILLAGE CENTER

District Overview

Paradise Cut Village Center is a mixed-use district located at the southwestern entrance to River Islands, with retail, commercial, office, higher density residential, entertainment, recreation, education, and possibly civic uses. The Village Center will feature pedestrian-friendly streets, walkable blocks, and a focus on water with a formal central lake. Land use will be flexible to respond to changing conditions which may occur as development proceeds.

In order to promote the character of the Village Center, a master developer will be required to prepare a NDP and AG/DS, or equivalent Paradise Cut Village Center Plan for review by the STDRC and approval of the City, that will include guidelines and standards to help create the pedestrian scale environment envisioned. For example, in areas where retail uses dominate, buildings will be required to occupy a significant portion of the property line adjacent to the street, with front doors that open to the street and transparent first floor elevations. Also, restaurants will be allowed to use a portion of the public sidewalk for seating, and awnings may encroach in the right-of-way. Such practices are all intended to encourage a traditional, pedestrian-friendly character. The Village Center will also include recreational uses in the form of public parks and open space that will connect to the lake and pedestrian corridors.

The illustrative plan shown in Figure 4.1 depicts the conceptual layout and overall character of the district as one of the many possible ways that the Village Center might be built that is consistent with the provisions of the WLSR, City of Lathrop Zoning Ordinance, and the UDC. The labels identify some of its main features, and specific details will be included in subsequent NDP and AG/DS, or equivalent Paradise Cut Village Center Plan documents.



LEGEND

- Pedestrian Connection using Sidewalks
- Multipurpose Levee Trails (Elevated)
- Class I Bike Trail
- Class II Bike Lanes / Detached Sidewalks
- Class III Bike Route
- Potential Pedestrian Connection through Parks and Paseos
- Village Entries
- District Gateway
- Community Gateway / Vehicular Bridge
- Pedestrian/Bike Bridge
- Build-to Line using Primary Building Orientation

Figure 4.2: District Guidelines



Example of a Formal Lake Feature with Seating and Lake Viewing Opportunities



A Shaded Outdoor Gathering and Seating Area with Decorative Lighting



Outdoor Seating and Dining in an Urban Mixed-use Setting with Wide Sidewalks



A Linear Water Feature to Frame the View and Adjacent Developments



Conceptual Illustration of Outdoor Activities and Development Character Around a Formal Lake Feature within Paradise Cut Village Center

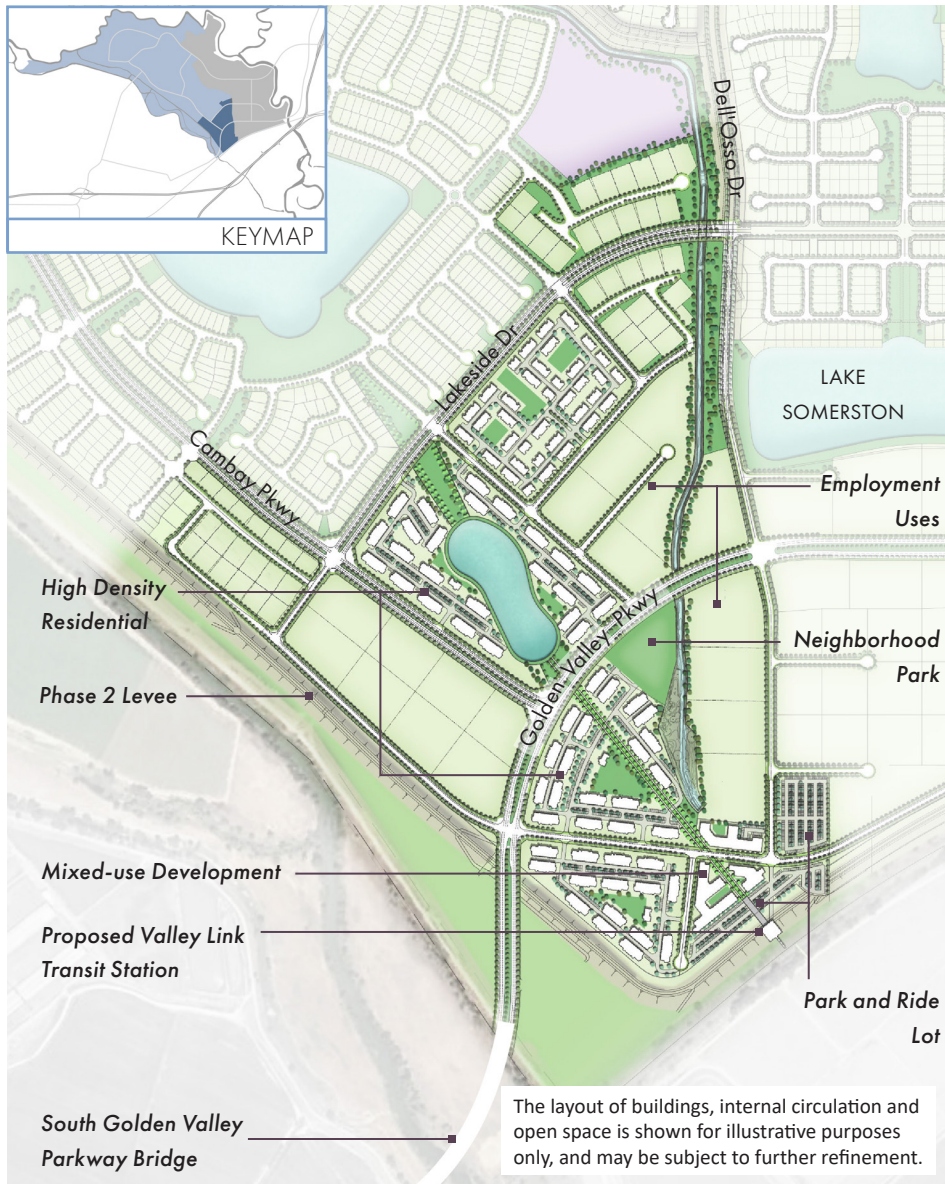


Figure 4.3: Employment Center/Transit Oriented Development Illustrative Plan

4.2: EMPLOYMENT CENTER/TRANSIT ORIENTED DEVELOPMENT

District Overview

Employment Center/Transit Oriented Development will be a mixed-use district intended to provide high density housing, service retail and restaurants, and high quality jobs near the proposed Valley Link station. The Valley Link transit service is expected to further increase the market capture of employment uses, and greatly improve commute times to the Bay Area for the River Islands residents.

Larger parcel sizes and block configurations for employment uses allow for flexibility in the development type and character, which may include Class A office buildings, research and development buildings, as well as light manufacturing/assembly facilities that are ancillary to other permitted uses. Parcels near Lake Somerston present access to lake views and recreational opportunities to its users.

The district may also include medical facilities and medical office buildings; educational and institutional facilities, churches; hotels; and other related uses that are compatible with the primary uses.

Landscape character will be the primary element providing a sense of unity for the district. Street trees located in parkways adjacent to the streets, as well as consistent street furniture and signs within the district, will contribute to creating the district's branding and identity.

The highlighted area of the illustrative plan (see Figure 4.3) shows the conceptual layout and overall character of the district as it might be developed in accordance with the standards for the Employment Center/Transit Oriented Development district. The labels identify some of the major features of the district. Specific details will be included in subsequent NDP and AG/DS, or equivalent Employment Center/Transit Oriented Development Plan documents.

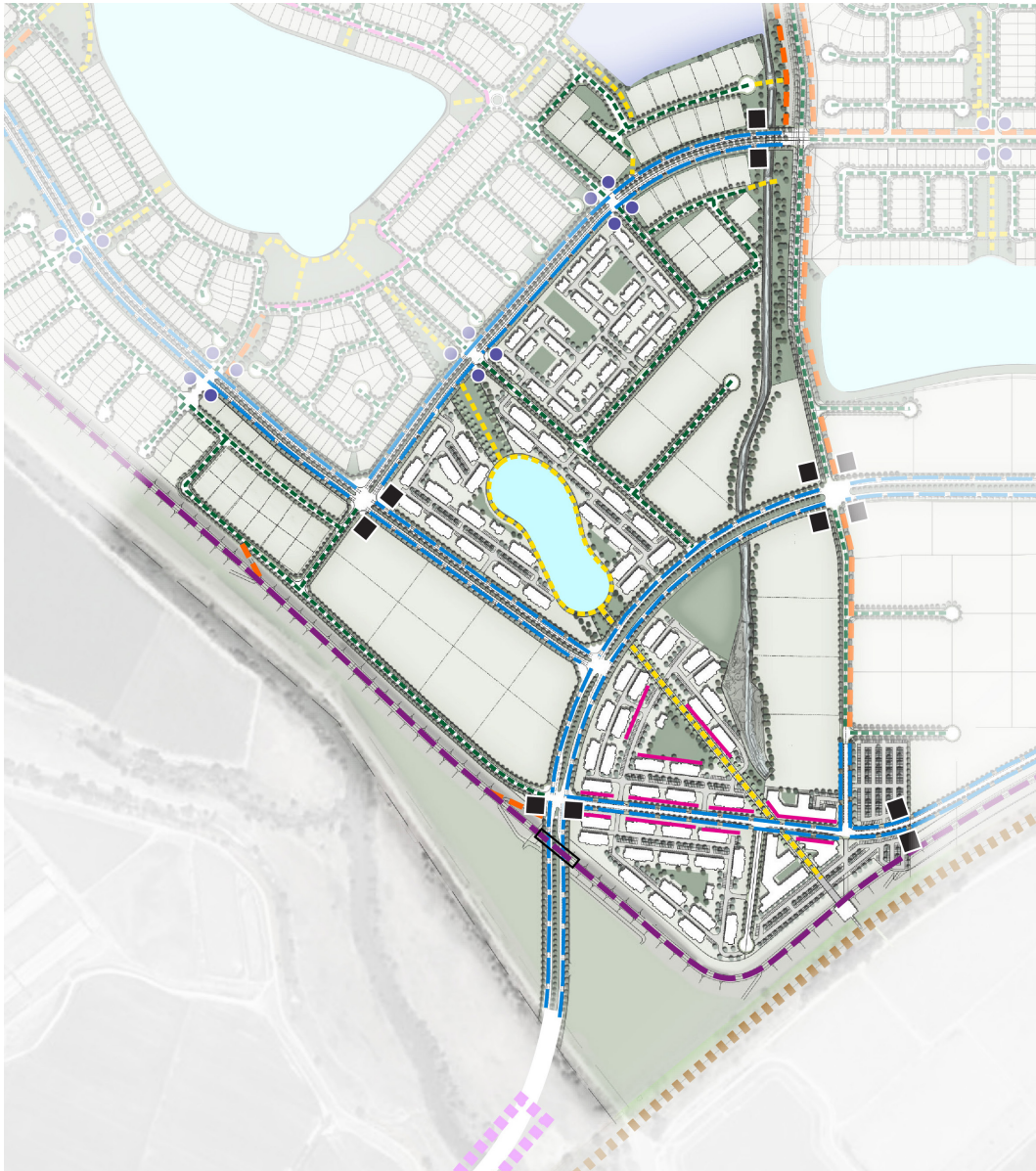


Figure 4.4: Employment Center/Transit Oriented Development District Guidelines

LEGEND

- - - Pedestrian Connection using Sidewalks
- - - Multipurpose Levee Trails (Elevated)
- - - Class I Bike Trail
- - - Class II Bike Lanes / Detached Sidewalks
- - - Class III Bike Route
- - - Potential Pedestrian Connection through Parks and Paseos
- ● Village Entries
- ■ District Gateway
- - - Community Gateway / Vehicular Bridge
- Pedestrian/Bike Bridge
- — Build-to Line using Primary Building Orientation



High Density Housing Near Rail Transit



Mixed-Use Development with Ground Floor Retail Surrounding an Urban Plaza



Employment Center Development Character with Larger Office Buildings surrounded by Open Space



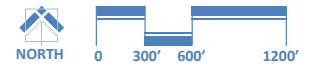
Bus Transfer Facility at the Train Station



Conceptual Illustration of Mixed-use Development Character in the Transit Oriented Development District



Figure 4.5: Woodlands Illustrative Plan



4.3: WOODLANDS

District Overview

Woodlands defines the western edge of River Islands and is bordered on the north and west by the Old River and on the south by Paradise Cut. This district may be divided into three sub-districts: North Woodlands, Central Woodlands and South Woodlands. This district features various densities of residential development, including larger single family lots, condominiums, and attached products. Woodlands also provides two K-8 schools, and strong pedestrian connectivity through numerous linear parks and open space corridors, eventually

interfacing with the more natural landscape of the Old River.

The labels on the illustrative plan (Figure 4.5) identify some of the major features of the district. A NDP and AG/DS will be prepared for either the entire district or may be phased into three sets of documents for each sub-district.



LEGEND

- Pedestrian Connection using Sidewalks
- Multipurpose Levee Trails (Elevated)
- Class I Bike Trail
- Class II Bike Lanes / Detached Sidewalks
- Class III Bike Route
- Potential Pedestrian Connection through Parks and Paseos
- Village Entries
- District Gateway
- Community Gateway / Vehicular Bridge
- Pedestrian/Bike Bridge
- Build-to Line using Primary Building Orientation

Figure 4.6: Woodlands District Guidelines



Figure 4.7: Old River Illustrative Plan

4.4: OLD RIVER

District Overview

The Old River district is comprised of single family and multifamily residential neighborhoods of varying densities arranged in a walkable block pattern. A linear open space spine which includes a bioretention swale and connects to the Old River riparian corridor will be one of the character defining elements of this residential district. The district will be served by River Islands Parkway with direct access to the future Paradise Cut Village Center. The district will also benefit from a neighborhood park constructed in Phase 1 of River Islands, providing spaces for outdoor gathering and play for the residents.

The district illustrative plan (Figure 4.7) shows the extent and overall character of the district, as well as some of its major features. A NDP and AG/DS document will provide development standards, architectural guidelines, and other details necessary for subsequent development.

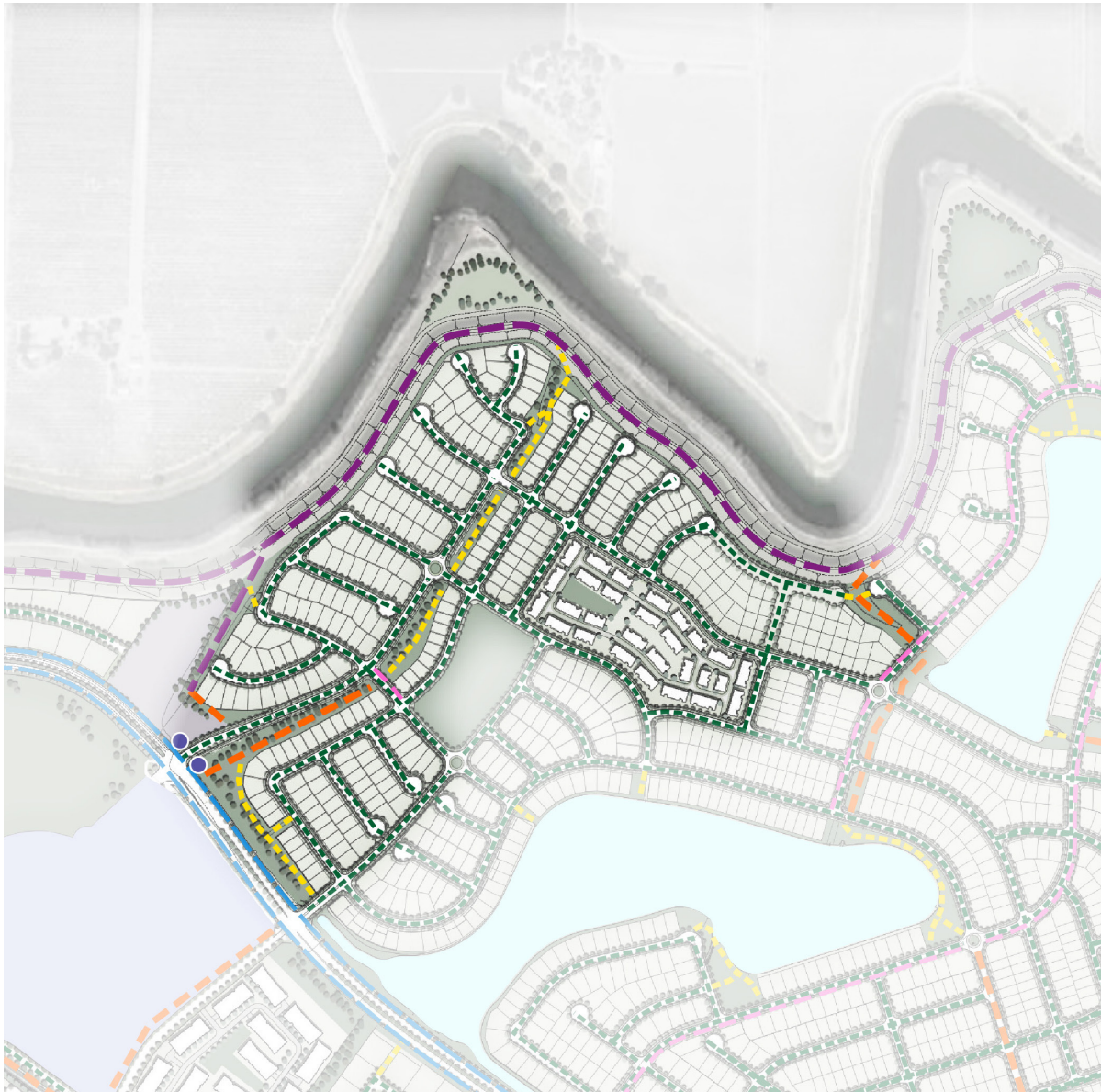


Figure 4.8: Old River District Guidelines

LEGEND

- - - Pedestrian Connection using Sidewalks
- - - Multipurpose Levee Trails (Elevated)
- - - Class I Bike Trail
- - - Class II Bike Lanes / Detached Sidewalks
- - - Class III Bike Route
- - - Potential Pedestrian Connection through Parks and Paseos
- ● Village Entries

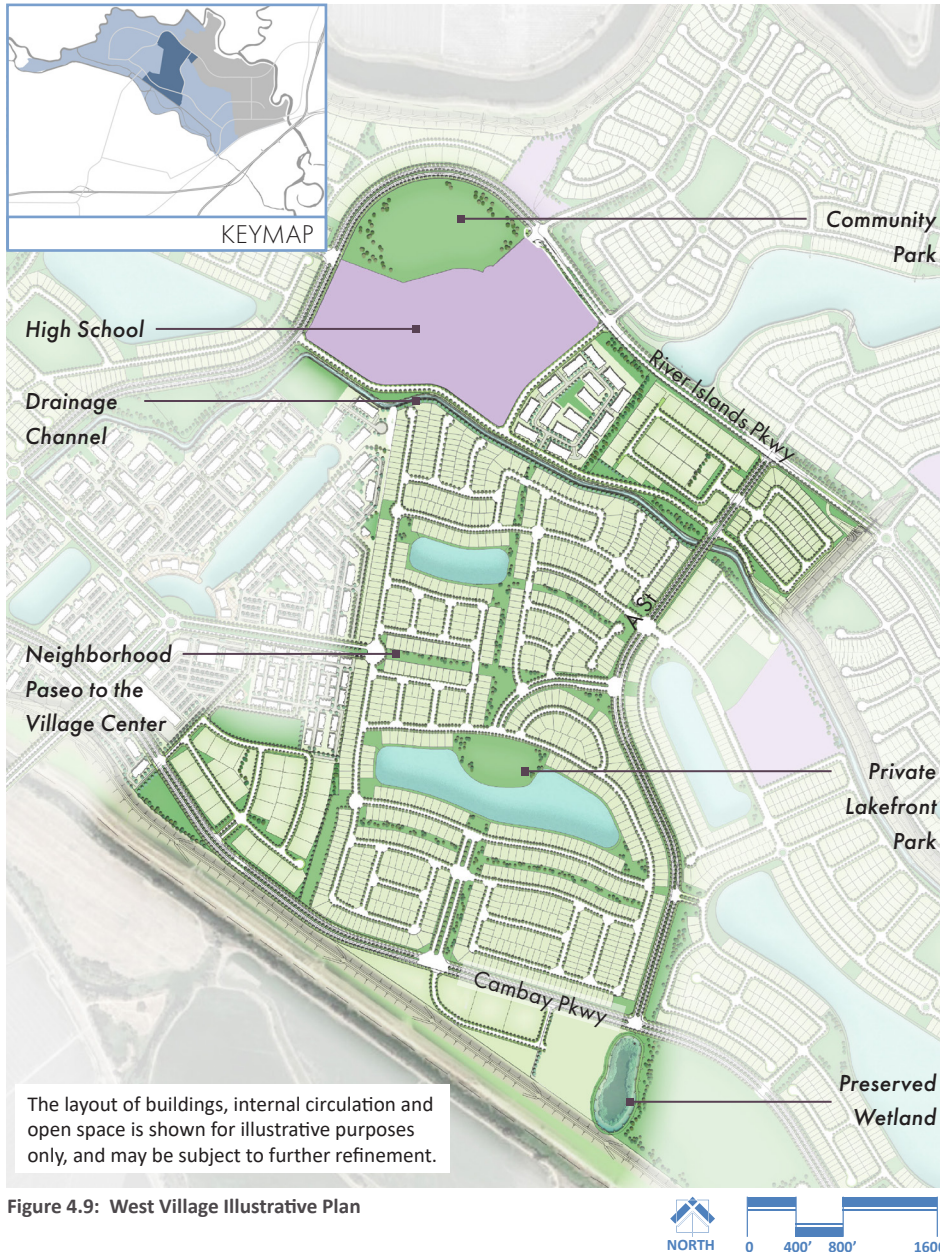


Figure 4.9: West Village Illustrative Plan

4.5: WEST VILLAGE

District Overview

The West Village is comprised of residential neighborhoods, lakes and open space that may accommodate one or more active adult communities. Active adult communities feature single story homes and other more accessible housing products with resort-style amenities geared towards homeowners 55 years and older. This district will also include River Islands High School and provide vehicular and pedestrian connectivity to the adjacent Paradise Cut Village Center.

The labels shown in Figure 4.9 identify some of the major features of the district. A NDP and AG/DS document will provide development standards, architectural guidelines, and other details necessary for subsequent development.

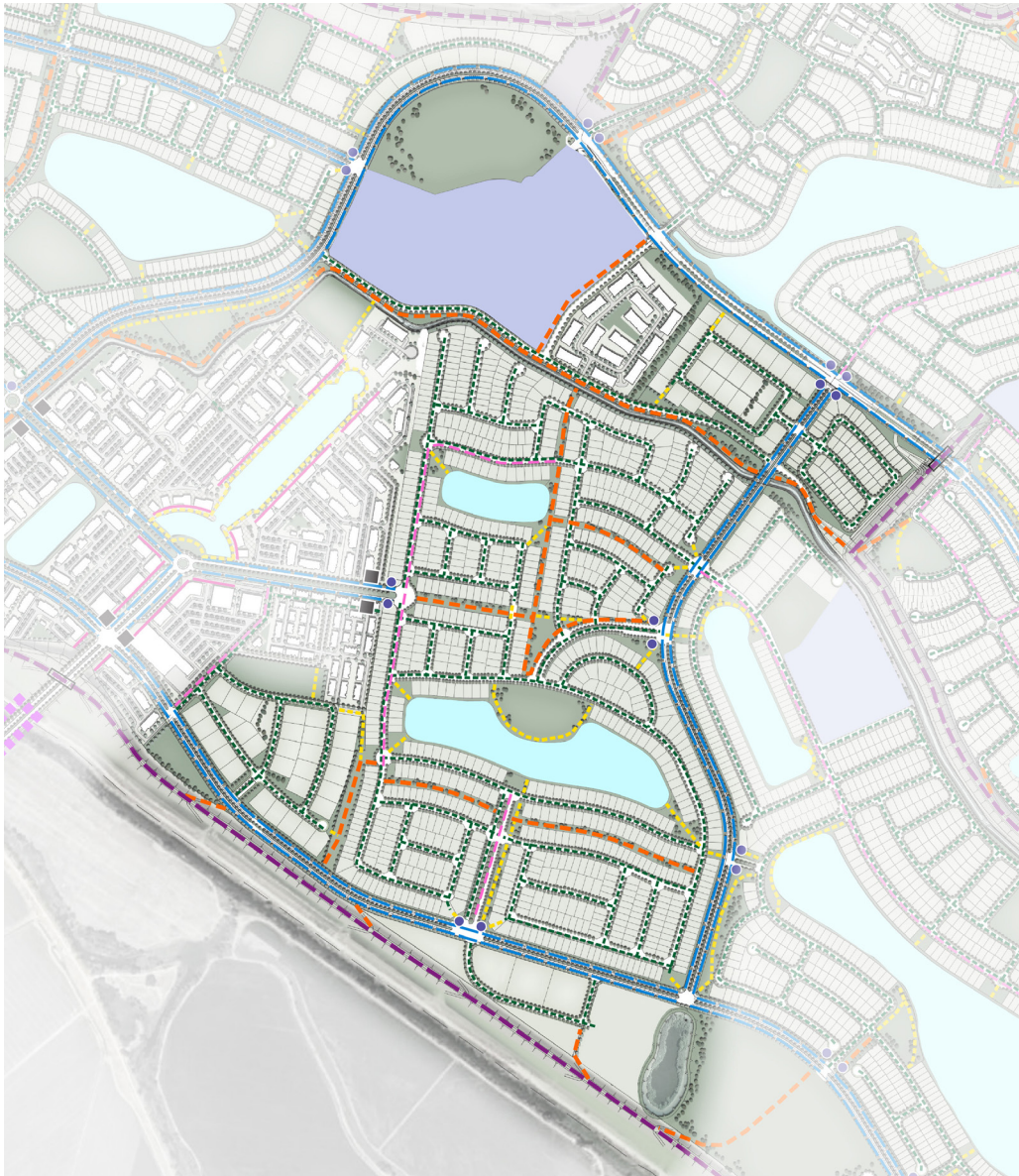


Figure 4.10: West Village District Guidelines

LEGEND

- - - Pedestrian Connection using Sidewalks
- - - Multipurpose Levee Trails (Elevated)
- - - Class I Bike Trail
- - - Class II Bike Lanes / Detached Sidewalks
- - - Class III Bike Route
- - - Potential Pedestrian Connection through Parks and Paseos
- Village Entries

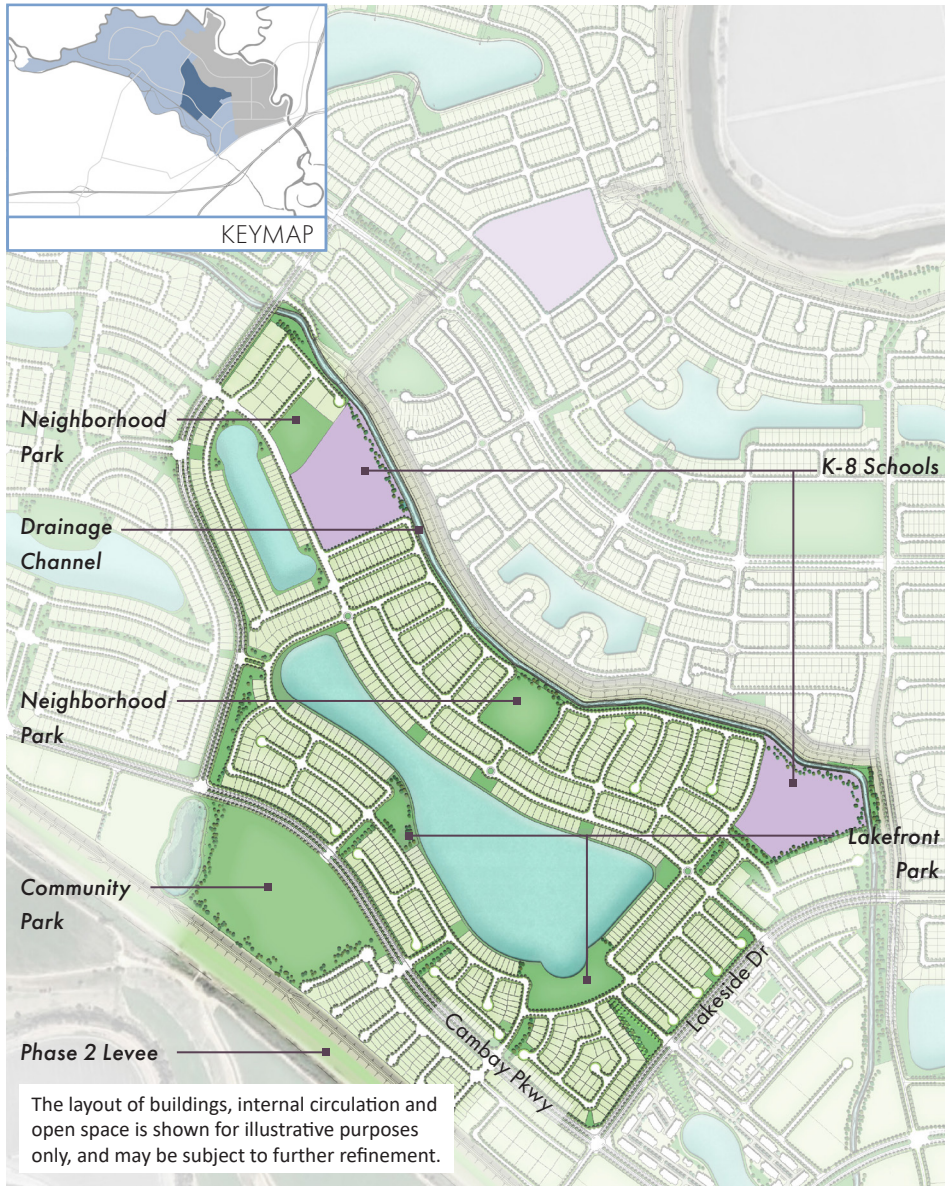
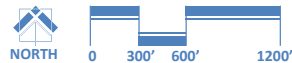


Figure 4.11: Lake Harbor Illustrative Plan

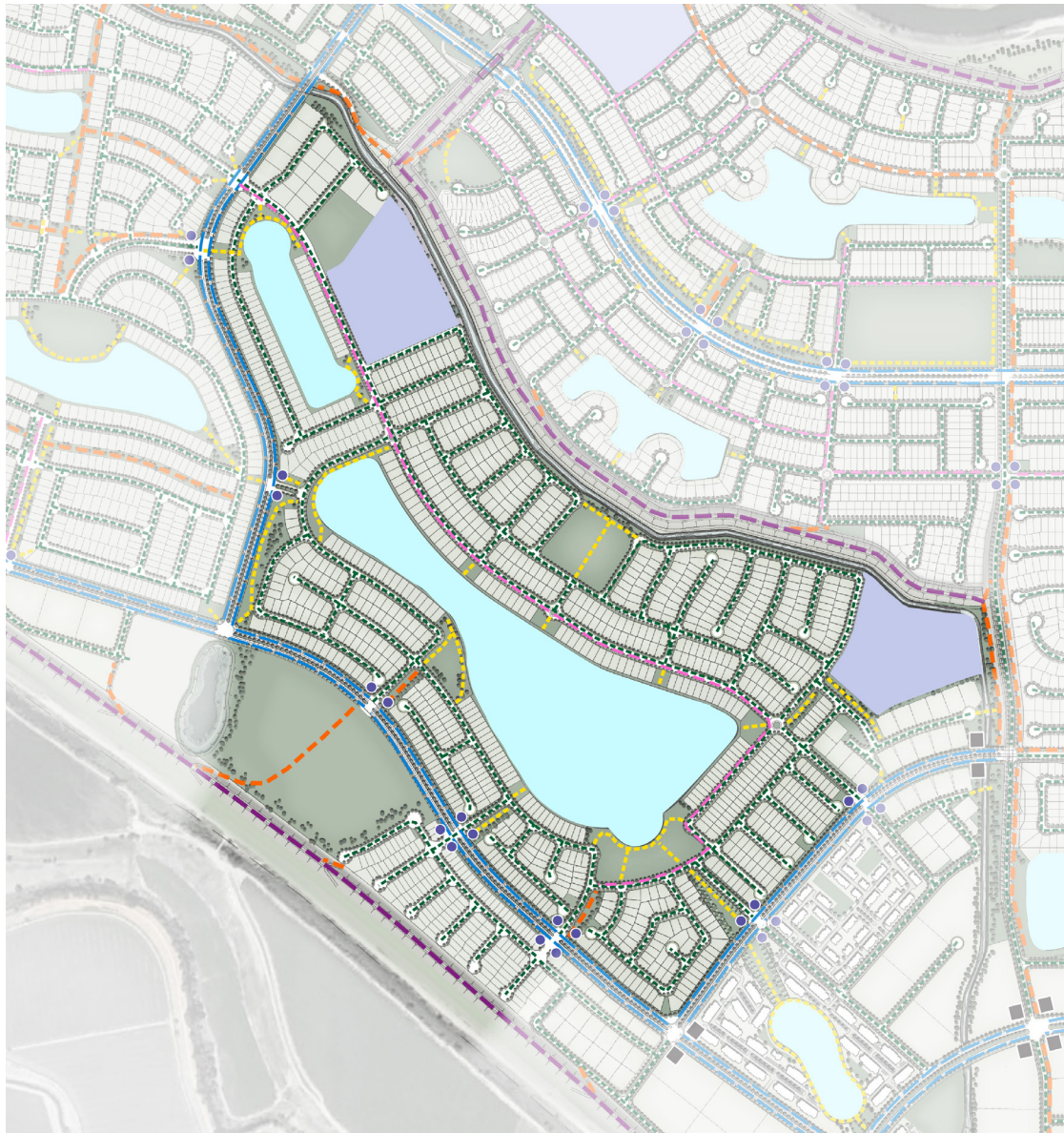


4.6: LAKE HARBOR

District Overview

The Lake Harbor district is a residential district featuring the largest lake within River Islands and will provide numerous opportunities for residential development to interface with the lake through trails, docks, parks and other open space elements.

The labels shown in Figure 4.11 identify some of the major features of the district. A NDP and AG/DS document will provide development standards, architectural guidelines, and other details necessary for subsequent development.



LEGEND

- - - Pedestrian Connection using Sidewalks
- - - Multipurpose Levee Trails (Elevated)
- - - Class I Bike Trail
- - - Class II Bike Lanes / Detached Sidewalks
- - - Class III Bike Route
- - - Potential Pedestrian Connection through Parks and Paseos
- ● Village Entries

Figure 4.12: Lake Harbor District Guidelines



APPENDIX

APPENDIX A: TYPICAL LOT DESIGN STANDARDS

The site plans on the following pages illustrate typical lot design and building placement standards. They are not intended to be exhaustive, however, and other building placements may be acceptable subject to review by STDRC.

A.1 Single Family Detached - Larger Lot Examples

Table A.1: Single Family Detached - Larger Lot Development Standards

Setbacks (minimum)	
Front Yard	
Living Space (First/Second Story)	(20'/25')
Porch (5' min. Depth)	15'
Garage Front Facing	25'
Side-Entry Garage Wall	15'
Front Courtyard walls	10'
Side Yard	
Living Space (Interior Property Line) (2)	5' min. 15' Aggregate
Living Space (Corner Property Line)	15'
Wrap Around Porch (Corner)	12'
Detached Garages/Accessory Unit	10'
Rear Yard	
Living Space (Min./Ave.)	(20'/25')
Front Entry Attached/Detached Garages	10'
Garages with Rear Access (1)	N/A
Patio Covers (1 Story 10' max. Height)	20' min
Building Coverage	45%

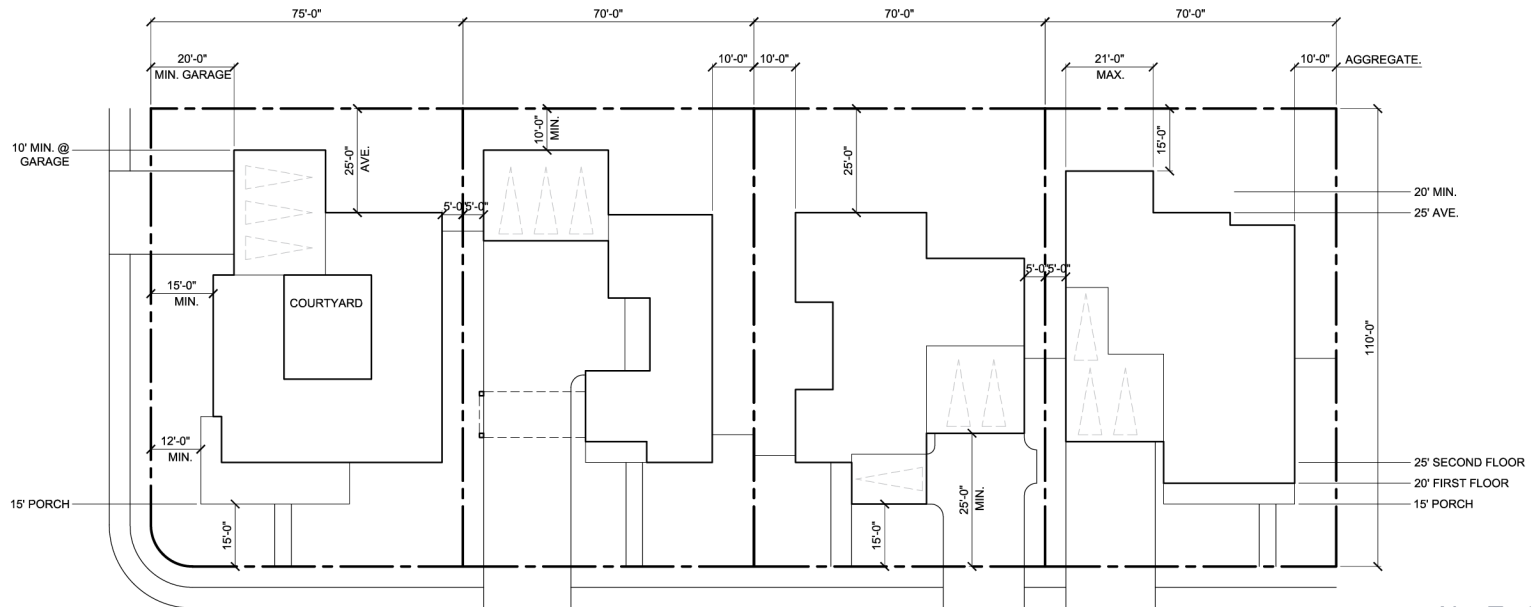


Figure A.1: Single Family Detached - Larger Lot Typical Layout

A.2 Single Family Detached - Medium Lot Examples

Table A.2: Single Family Detached - Medium Lot Development Standards

Setbacks (minimum)	
Front Yard	
Living Space (First/Second Story)	(15'/20')
Porch (5' min. Clear Depth)	12'
Garage Front Facing (Door)	20'
Side-Entry Garage Wall	N/A
Front Courtyard walls	10'
Side Yard	
Living Space (Interior Property Line)	5'
Living Space (Corner Property Line)	10'
Wrap Around Porch (Corner)	7'
Detached Garages/Accessory Unit	5'
Rear Yard	
Living Space (Min./Ave.) ⁽¹⁾	(15'/20')
Front Entry Attached/Detached Garages ⁽²⁾	N/A
Garages with Rear Access	N/A
Patio Covers (1 Story Height)	10'
Building Coverage	50%

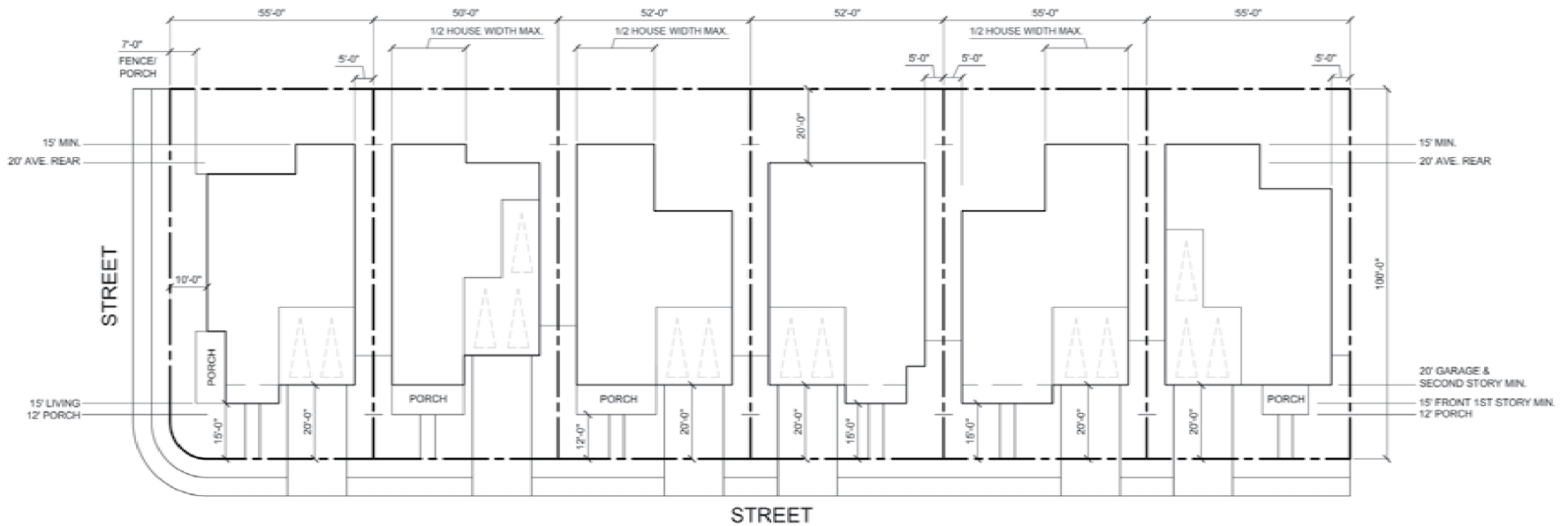


Figure A.2: Single Family Detached - Medium Lot Typical Layout

A.3 Single Family Detached - Small Lot Examples

Table A.3: Single Family Detached - Small Lot Development Standards

Setbacks (minimum)	
Front Yard	
Living Space (First/Second Story)	(14'/16')
Porch (5' min. Clear Depth)	10'
Garage Front Facing (Door)	18'
Side-Entry Garage Wall	Not Allowed
Front Courtyard walls	10'
Side Yard	
Living Space (Interior Property Line)	4'
Living Space (Corner Property Line)	9'
Wrap Around Porch (Corner)	7'
Detached Garages/Accessory Unit	N/A
Rear Yard	
Living Space (Min./Ave.) ⁽¹⁾	(15'/20')
Front Entry Attached/Detached Garages ⁽²⁾	5'
Garages with Rear Access	N/A
Patio Covers (1 Story Height)	10' min
Building Coverage	55%

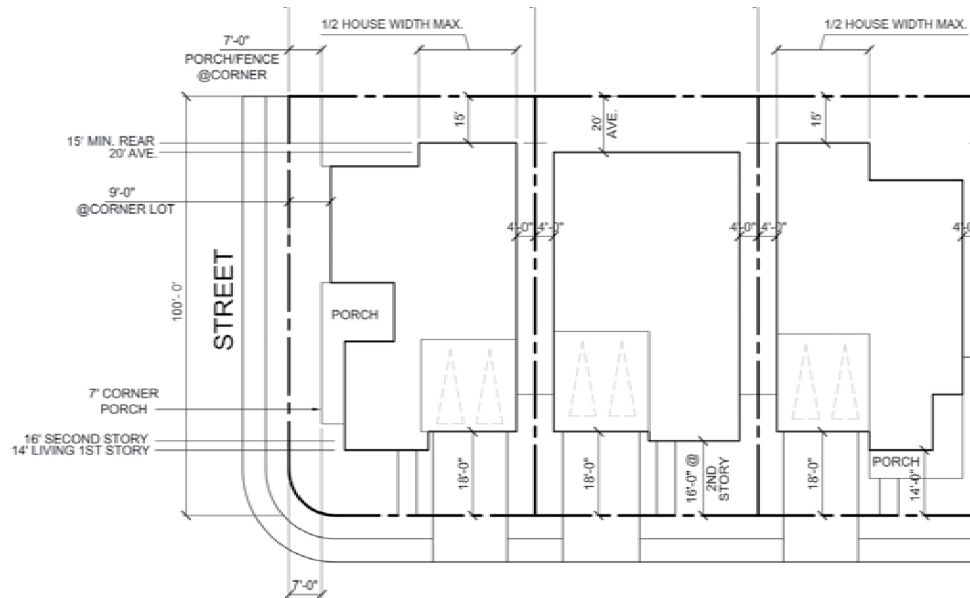


Figure A.3: Single Family Detached - Small Lot Typical Layout

A.4 Medium Density Residential - Bungalows

Table A.4: Medium Density Residential - Bungalows Development Standards

Setbacks (Minimum)		
Front Yard @ Street		
Living Space (First/Second Story)	(10'/10')	(10'/10')
Porch (6' min. clear depth)	6'	6'
Garage Front Facing (Door)	N/A	N/A
Side-Entry Garage Wall	N/A	N/A
Front walls (Private Street)	2'	2'
Front Yard @ Non-Street Frontage		
Living Space (First/Second Story)	(5'/5')	(5'/5')
Porch (6' min. clear depth)	6'	6'
Garage Front Facing (Door)	N/A	N/A
Side-Entry Garage Wall	5'	5'
Front walls (Private Street)	2'	2'

Setbacks (Minimum)		
Rear Yard		
Living Space (Min./Ave) - See Note 6	10'	10'
Front Entry Attached/Detached Garages	2'	N/A
Garages with Rear Access	Not Allowed	Not Allowed
Patio Covers (1 Story Height)	Not Allowed	Not Allowed
Alley / Private Drive		
Garage (door)	2' Apron	2' Apron
Garage (to center of Private Drive)	14'	14'
Lot Coverage		N/A
Structure Height (Maximum)		35'

Side Yard		
Living Space (Interior Property Line)	4'	4'
Living Space (Corner Property Line)	N/A	N/A
Wrap Around Porch (Corner)	Not Allowed	Not Allowed
Detached Garages/Accessory Unit	Not Allowed	Not Allowed

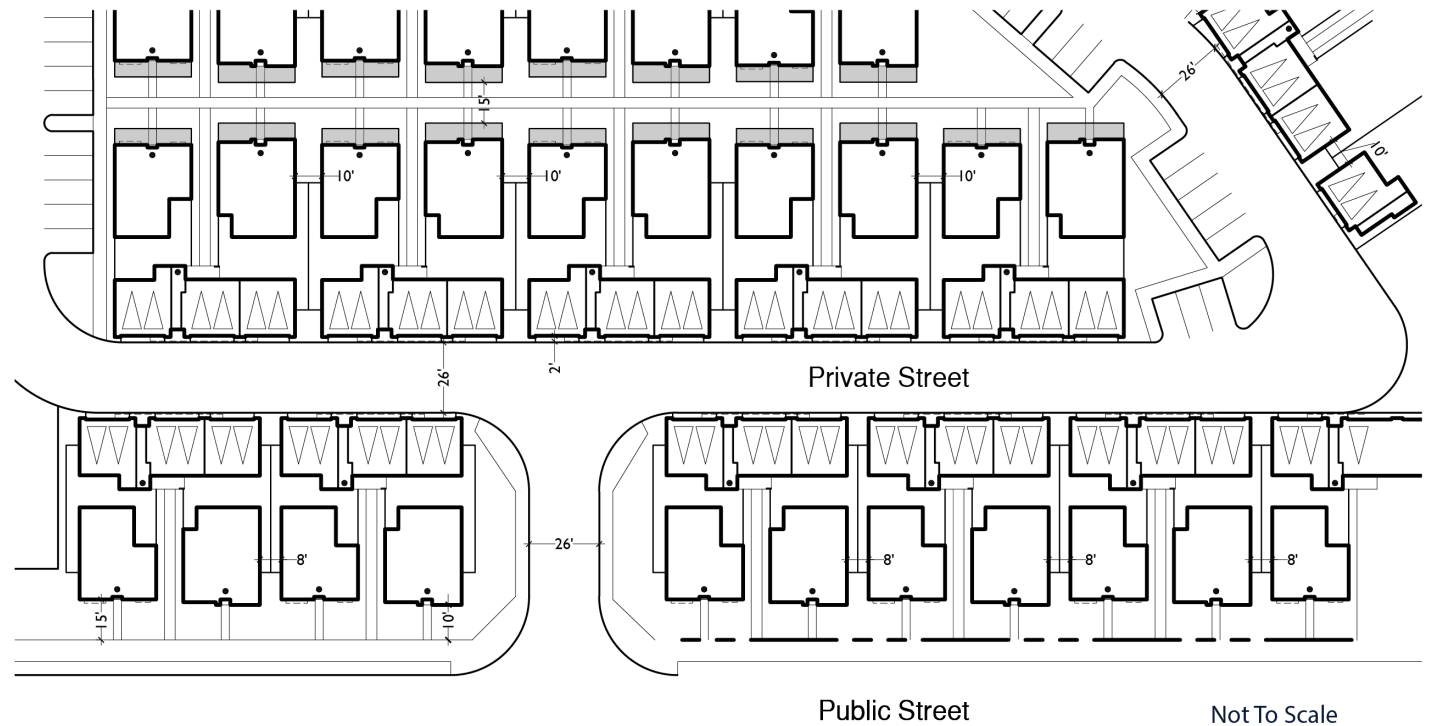


Figure A.4: Medium Density Residential - Bungalows Typical Layout

A.5 Medium Density Residential - Courtyards

Table A.5: Medium Density Residential - Courtyards Development Standards

Setbacks (Minimum)		
Front Yard @ street		
Living Space (First/Second Story)	(14'/14')	(14'/14')
Porch (6' min. clear depth)	8'	8'
Garage Front Facing (Door)	N/A	N/A
Side-Entry Garage Wall	N/A	N/A
Front Courtyard walls	8'	8'
Side Yard		
Living Space (Interior Property Line)	4'	4'
Living Space (Corner Property Line)	N/A	N/A
Wrap Around Porch (Corner)	N/A	N/A
Detached Garages/Accessory Unit	N/A	N/A

Setbacks (Minimum)		
Rear Yard		
Living Space (Min./Ave)	10'	10'
Front Entry Attached/Detached Garages	N/A	N/A
Garages with Rear Access (Apron)	4'	N/A
Patio Covers (1 Story Height)	N/A	N/A

Setbacks (Minimum)		
Alley / Private Drive		
Garage (door)	4' Apron	4' Apron
Living space (to center of Alley)	14'	14'
Paseo Walk		
Living Space (to enter of Paseo)	10'	N/A
Porch (to center of Paseo)	8'	N/A
Porch (see Green Court example)	N/A	8'
Building Coverage		
	N/A	N/A

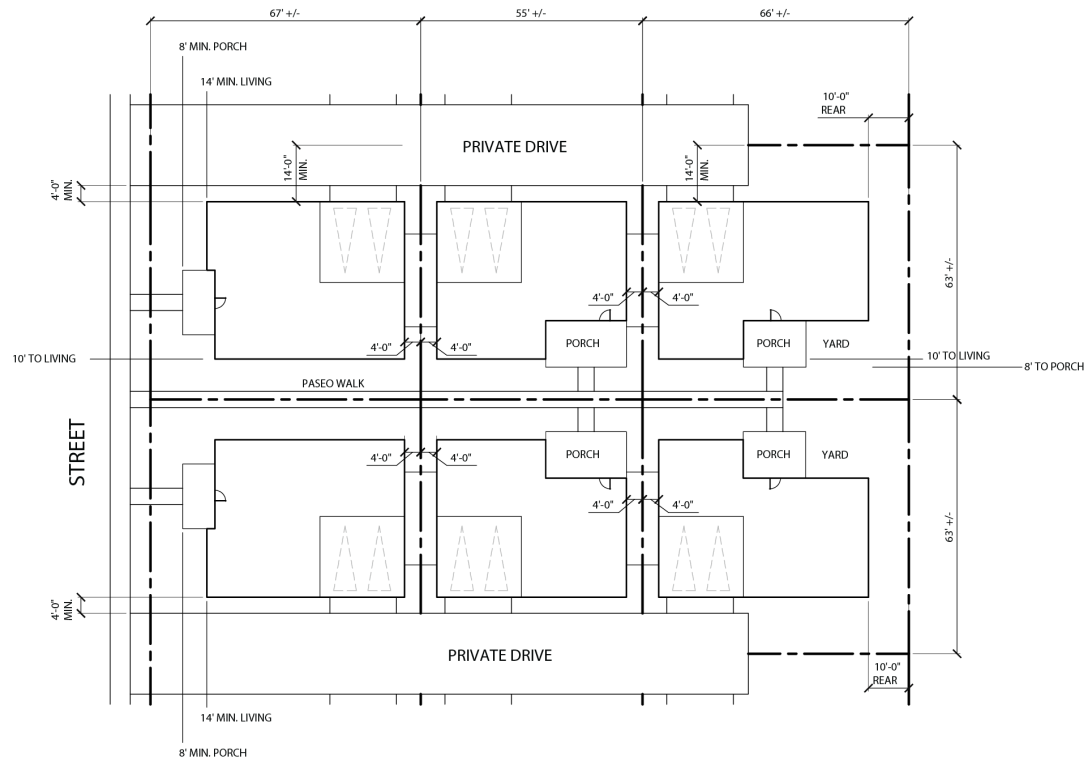


Figure A.5: Medium Density Residential - Courtyards Typical Layout

